

Department of Transportation

NY ROUTE 74 OVER EAGLE LAKE CHANNEL CULVERT REPLACEMENT S.H. 8230 TOWN OF TICONDEROGA

FEDERAL AID PROJECT

THE LATEST REVISIONS OF THE STANDARD SHEETS MAINTAINED BY THE DEPARTMENT, WHICH ARE CURRENT ON THE DATE OF ADVERTISEMENT FOR BIDS, SHALL BE CONSIDERED TO BE IN EFFECT. ALL PAY ITEMS AND WORK CONTAINED IN THE CONTRACT AND ANY ADDITIONAL PAY ITEMS AND WORK ENCOUNTERED DURING THE COURSE OF THE CONTRACT SHALL BE SUBJECT TO THE APPLICABLE STANDARD SHEETS UNLESS OTHERWISE SPECIFIED IN THE CONTRACT DOCUMENTS.

ALL WORK CONTEMPLATED UNDER THIS CONTRACT IS TO BE COVERED BY AND IN CONFORMITY WITH THE STANDARD SPECIFICATIONS (US CUSTOMARY) REFERENCED IN THE CONTRACT PROJECT "PROPOSAL" EXCEPT AS MODIFIED BY THESE PLANS OR BY CHANGES SET FORTH IN THE CONTRACT PROJECT "PROPOSAL."

CONTRACT PLANS HAVE BEEN DESIGNED IN ACCORDANCE WITH NYS DOT POLICIES AND GUIDELINES AND THE FINAL DESIGN REPORT APPROVED ON 3/15/2022

PROJECT DESCRIPTION

PROJECT INVOLVES THE REPLACEMENT OF AN EXISTING CULVERT ON NY ROUTE 74 AT EAGLE LAKE CHANNEL.

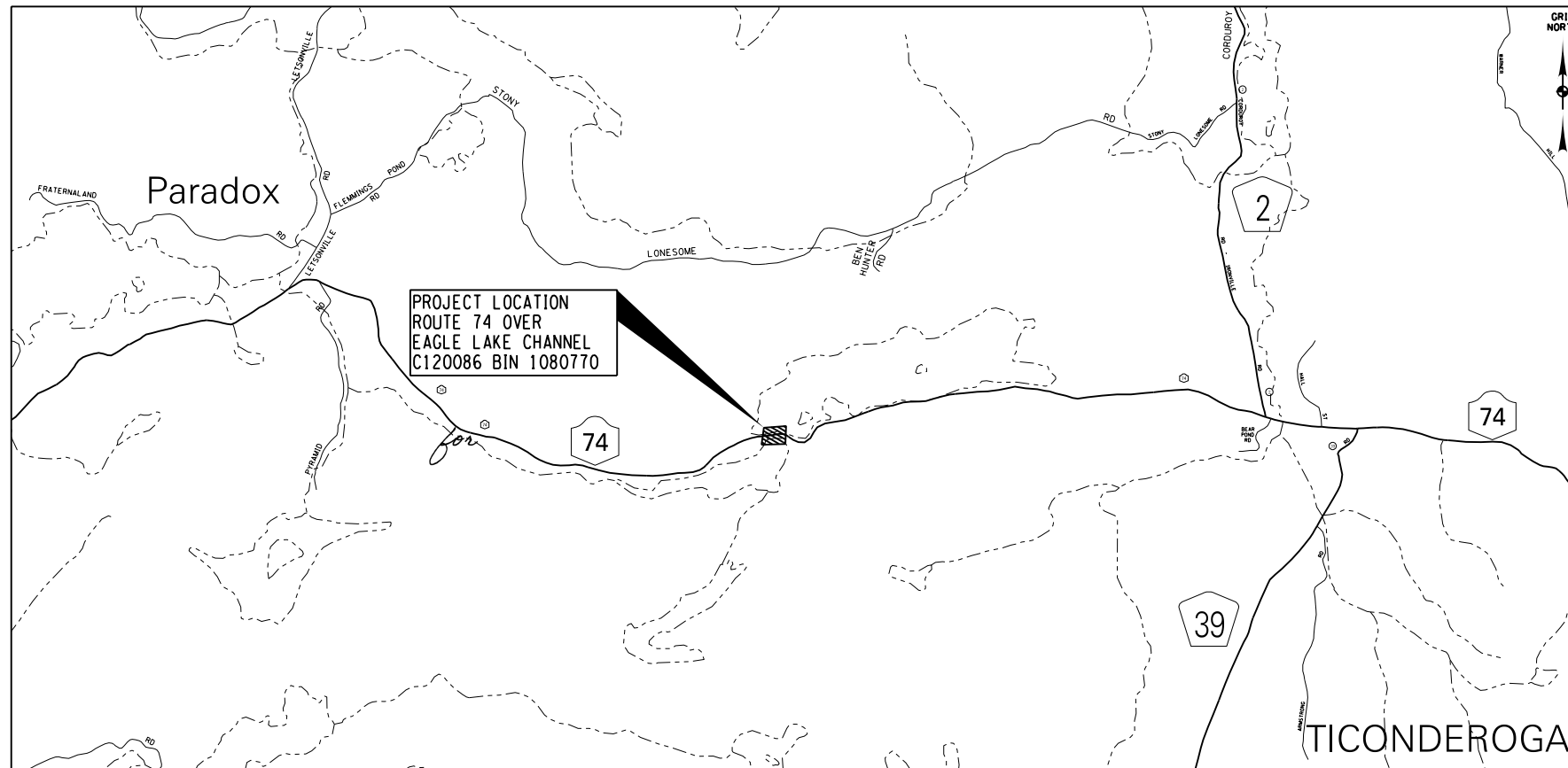
CONTRACT LIMITS

CONTRACT LIMITS FOR ALL SITES AND ALL INTERSECTING ROADWAYS THAT CONTAIN CONSTRUCTION SIGNAGE BEGIN 500 FT PRIOR TO THE FIRST ADVANCED WARNING SIGN (INCLUDING ALL PORTABLE AND FIXED VARIABLE MESSAGE SIGNS) AND 500 FT BEYOND THE END ROADWORK SIGNING.

CONTRACT D264836

ESSEX COUNTY

CONTRACTOR'S NAME	_____
AWARD DATE	_____
COMPLETION DATE	_____
FINAL ACCEPTANCE DATE	_____
REGIONAL DIRECTOR	_____
ENGINEER IN CHARGE	_____
FINAL COST TOTAL	_____
FISCAL SHARE	COST(S)



PROJECT LOCATION

RECOMMENDED BY
Jeffrey Pangburn 4/20/22
CONSULTANT DESIGN ENGINEER DATE
JEFFREY PANGBURN, P.E.

RECOMMENDED BY
Michael R. Cukrovany 4/20/2022
REGIONAL DESIGN ENGINEER DATE
MICHAEL R. CUKROVANY, P.E.

RECOMMENDED BY
Craig T. Blake 4/20/2022
REGIONAL CONSTRUCTION ENGINEER DATE
CRAIG T. BLAKE, P.E.

RECOMMENDED BY
Christopher F. Ward 4/21/22
REGIONAL DIRECTOR OF OPERATIONS DATE
Christopher Ward, P.E., Albany County Resident Engineer
for STEPHEN C. CLINTON, P.E.

RECOMMENDED BY
Mark A. Pyskadlo 4/21/2022
REGIONAL TRAFFIC ENGINEER DATE
MARK A. PYSKADLO, P.E.

APPROVED BY
Patrick S. Barnes 04/26/2022
REGIONAL DIRECTOR DATE
PATRICK S. BARNES, P.E.



CULVERT REPLACEMENT PROJECT		
NY ROUTE 74 OVER EAGLE LAKE CHANNEL		
S.H. 8230		
COUNTY: ESSEX COUNTY		
FED. ROAD REG. NO.	STATE	SHEET NO.
1	N.Y.	1
CAPITAL PROJECT IDENTIFICATION NO. 1110.02		

INDEX ON SHEET NO. 2

FILE NAME = N:\Projects\2020\126-209 NYS001 - Eagle Lake\Working\CADD\ dgn\111002.cpl.mxd.dgn
 DATE/TIME = 4/27/2022 2:28:34 PM
 USER = KDetrick
 PLOT = NYS001.DWG.PDF.plt:fg

DESIGN SUPERVISOR J. PANGBURN JOB MANAGER C. GEROUX DESIGN L. SHORT DRAFTING K. DETRICK CHECK A. BROWN CHECK A. BROWN PROJECT MANAGER C. GEROUX

ALIGNMENT		TOPOGRAPHY (MISCELLANEOUS)		UTILITIES	
ABBR.	DESCRIPTION	ABBR.	DESCRIPTION	ABBR.	DESCRIPTION
AH	AHEAD	ABUT	ABUTMENT	E	ELECTRIC
AZ	AZIMUTH	AOBE	AS ORDERED BY ENGINEER	EMH	ELECTRIC MANHOLE
BK	BACK	ASPH	ASPHALT	G	GAS
B	BASELINE	BDY	BOUNDARY	GP	GUY POLE
BRG	BEARING	BLDG	BUILDING	GSB	GAS SERVICE BOX (HOUSE LINE)
C	CENTERLINE	BM	BENCH MARK	GV	GAS VALVE (MAIN LINE)
CS	CURVE TO SPIRAL	CC	CENTER TO CENTER	HYD	HYDRANT
e	SUPERELEVATION RATE (CROSS SLOPE)	CONC	CONCRETE	LP	LIGHT POLE
EQ	EQUALITY	CONST	CONSTRUCTION	LPG	LOW PRESSURE GAS
EXT	EXTERNAL	CR	COUNTY ROAD	PP	POWER POLE
HCL	HORIZONTAL CONTROL LINE	D	DEED DISTANCE	SA	SANITARY SEWER
HSD	HEADLIGHT SIGHT DISTANCE	DM	DIRECT MEASUREMENT	SMH	SANITARY MANHOLE
L	LENGTH OF CIRCULAR CURVE	DWY	DRIVEWAY	ST	STORM SEWER
LS	LENGTH OF SPIRAL	EP	EDGE OF PAVEMENT	T	TELEPHONE
LVC	LENGTH OF VERTICAL CURVE	ES	EDGE OF SHOULDER	TCB	TRAFFIC CONTROL BOX
E	CENTER CORRECTION OF VERTICAL CURVE	FEE	FEE ACQUISITION	TELBOX	TELEPHONE BOX
M	MAIN LINE	FEE WO/A	FEE ACQUISITION WITHOUT ACCESS	TEL P	TELEPHONE POLE
PC	POINT OF CURVATURE	FP	FENCE POST	TMH	TELEPHONE MANHOLE
PI	POINT OF INTERSECTION	FD	FOUNDATION	CTV	CABLE TELEVISION
POL	POINT ON LINE	FL	FENCE LINE	W	WATER
PSD	PASSING SIGHT DISTANCE	GAR	GARAGE	WSB	WATER SERVICE BOX (HOUSE LINE)
PT	POINT OF TANGENT	GR	GRAVEL	WV	WATER VALVE (MAIN LINE)
PVC	POINT OF VERTICAL CURVE	HO	HOUSE	SUBSURFACE EXPLORATION	
PVI	POINT OF VERTICAL INTERSECTION	HWY	HIGHWAY		
PVT	POINT OF VERTICAL TANGENT	IP	IRON PIN OR IRON PIPE	ABBR.	DESCRIPTION
R	RADIUS	MB	MAILBOX	REPLACE ABBREVIATION "AB" WITH:	
SC	SPIRAL TO CURVE	MON	MONUMENT	AH	HAND AUGER
SSD	STOPPING SIGHT DISTANCE	N&W	NAIL AND WASHER	CP	CONE PENETROMETER
ST	SPIRAL TO TANGENT	OG	ORIGINAL GROUND	DA	2 1/4 INCHES CASED DRILL HOLE
STA	STATION	O/H	OVERHEAD	DM	DRILLING MUD
T	TANGENT LENGTH	P	PARCEL	DN	4 INCHES CASED DRILL HOLE
TGL	THEORETICAL GRADE LINE	PAV'T	PAVEMENT	FH	HOLLOW FLIGHT AUGER
TS	TANGENT TO SPIRAL	PE	PERMANENT EASEMENT	PA	POWER AUGER
VC	VERTICAL CURVE	PED POLE	PEDESTRIAN POLE	PH	PROBE
TOPOGRAPHY (DRAINAGE)		R	PROPERTY LINE	PT	PERCOLATION TEST HOLE
		POR	PORCH	RP	1 INCH SAMPLER (RETRACTABLE PLUG)
BB	BOTTOM OF BANK (STREAM)	RR	RAILROAD	TO BE DEFINED AT THE TIME OF EXPLORATION	
BC	BOTTOM OF CURB	RTE	ROUTE	SP	SEISMIC POINT
BO	BOTTOM OF OPENING	ROW	RIGHT OF WAY	TP	TEST PIT
CAP	CORRUGATED ALUMINUM PIPE	RW	RETAINING WALL	ABBREVIATION "C" IN CATEGORIES: DA, DM, DN, AND FH WITH:	
CB	CATCH BASIN	SH	STATE HIGHWAY	B	BRIDGE
CIP	CAST IRON PIPE	SHLDR	SHOULDER	C	CUT
C STRM	CENTERLINE OF STREAM	SPK	SPIKE	D	DAM
CMP	CORRUGATED METAL PIPE	ST	STREET	F	FILL
CP	CONCRETE PIPE	STK	STAKE	K	CULVERT
CSP	CORRUGATED STEEL PIPE	STY	STORY	W	WALL
CULV	CULVERT	SW	SIDEWALK	X	TO BE USED IF ONE OF THE ABOVE CANNOT BE DEFINED AT THE TIME THE EXPLORATION IS MADE
DIA	DIAMETER	TE	TEMPORARY EASEMENT		
DMH	DRAINAGE MANHOLE	TO	TEMPORARY OCCUPANCY		
DS	DRAINAGE STRUCTURE PIPE	U/G	UNDERGROUND		
D'XING	DITCH CROSSING	WW	WING WALL		
EHW	EXTREME HIGH WATER				
EL	ELEVATION				
ELEV	ELEVATION				
ELW	EXTREME LOW WATER				
ES	END SECTION				
HW	HEADWALL				
INV	INVERT				
MH	MANHOLE				
MHW	MEAN HIGH WATER				
OHW	ORDINARY HIGH WATER				
OLW	ORDINARY LOW WATER				
RCP	REINFORCED CONCRETE PIPE				
SICPP	SMOOTH INTERIOR CORRUGATED POLYETHYLENE PIPE				
TB	TOP OF BANK (STREAM)				
TC	TOP OF CURB				
TG	TOP OF GRATE				
VCP	VITRIFIED CLAY PIPE				

STANDARD SYMBOL (PLANS)	ITEM PAYMENT UNIT: ESTIMATE OF QUANTITIES SHEET	EQUIVALENT NOMENCLATURE: (SPECS/PROPOSAL)
"	-	INCHES
'	LF	LINEAR FEET
mi	MI	MILES
ft ²	SF	SQUARE FEET
YD ²	SY	SQUARE YARD
AC	AC	ACRES
YD ³	CY	CUBIC YARD
GAL	GAL	GALLON
lb	LB	POUND
TON	TON	TON

INDEX		TOTAL NUMBER OF SHEETS: 56
SHEET NUMBER	DESCRIPTION	DRAWING NUMBER
1	TITLE SHEET	COVER
2	INDEX AND ABBREVIATIONS	INDEX
3 TO 4	LEGEND, LINE AND POINT SYMBOLOLOGY	LEG-01 TO LEG-02
5	TYPICAL SECTIONS	TYP-01
6 TO 7	WORK ZONE TRAFFIC CONTROL NOTES	WZN-01 TO WZN-02
8 TO 15	WORK ZONE TRAFFIC CONTROL PLANS	WZP-01 TO WZP-08
16 TO 18	MISCELLANEOUS TABLES	MST-01 TO MST-03
19	MAINTENANCE JURISDICTION PLAN	MJP-01
20	EROSION AND SEDIMENT CONTROL PLAN	ECP-01
21	GENERAL PLAN	GNP-01
22 TO 23	UTILITY PLANS	UTP-01 TO UTP-02
24 TO 56	BRIDGE PLANS	ST-01 TO ST-33

NY ROUTE 74 OVER EAGLE LAKE CHANNEL	PIN 1110.02	BRIDGES 1080770	CULVERTS C120086	ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED	CONTRACT NUMBER D264836
CULVERT REPLACEMENT	UTILITY QUALITY LEVEL "C"			INDEX AND ABBREVIATIONS	DRAWING NO. INDEX SHEET NO. 2
SH 8230 SCHROON LAKE - TICONDEROGA, PART 2					
TOWN OF TICONDEROGA					
COUNTY: ESSEX COUNTY	REGION: 1				

ALIGNMENT			DRAINAGE			ITS			ROW MAPPING			SIGNS			UTILITIES		
CELL	NAME	DESCRIPTION	CELL	NAME	DESCRIPTION	CELL	NAME	DESCRIPTION	CELL	NAME	DESCRIPTION	CELL	NAME	DESCRIPTION	CELL	NAME	DESCRIPTION
⊕	ACC	CENTER OF CURVATURE	+	DINV	INVERT	⊕	IANT P	ANTENNAS	⊕	MDL1P	DEED LINE, TYPE 1	⊕	S	SINGLE POST	⊕	UEB	ELECTRIC, BOX
+	ACOGO	COGO	▭	DS	STRUCTURE, RECTANGULAR	⊕	IASCTS	ACCOU. SPEED/COUNT SNSR.S	⊕	MDL2P	DEED LINE, TYPE 2	⊕	S.P	SINGLE POST, PROPOSED	⊕	UEM	ELECTRIC, METER
⊙	ACS	CURVE TO SPIRAL	+	DSI	STRUCTURE, INVERT	⊕	ICABPAD	CABINET & PAD	⊕	MDL3P	DEED LINE, TYPE 3	⊕	SB.P	BACK TO BACK, PROPOSED	⊕	UEMH	ELECTRIC, MANHOLE
△	ADPL.P	DETOUR, POINT OF INTERSECT.	⊕	DSM	STRUCTURE, MANHOLE	⊕	ICCTV	CCTV SITE	⊕	MDL4P	DEED LINE, TYPE 4	⊕	SDEL	DELINEATORS	⊕	UEPT	ELECTRIC, POLE, TRANS.
⊙	ADPL.P	DETOUR, POINT ON LINE	⊕	DSMTXX.P	STRUCTURE, MANHOLE, TYPE "XX" "XX" = 48, 60, 72, 96	⊕	ICDPD	CDPD TRANSCEIVER	⊕	MDL5P	DEED LINE, TYPE 5	⊕	SPM	PARKING METER	⊕	UGM	GAS, METER
⊙	AEQN	EQUATION	⊕	DSR	STRUCTURE, ROUND	⊕	ICELLT	CELL PHONE TOWER	⊕	MEEP	EASEMENT, EXISTING	⊕	SRM	REFERENCE MARKERS	⊕	UGMH	GAS, MANHOLE
⊕	AEQNAHD	EQUATION AHEAD	⊕	DST"X"CB P	STRUCTURE, RECT., WITH CURB TYPE "X" "X" = F, G, N, O, P, R	⊕	ICJB	CONDUIT JACK OR BORING	⊕	MEPAP.P	EASEMENT, PERM., APPROX.	⊕	SRSC3	SHLD, CTY, 123 DIG.	⊕	UGLM	GAS, LINE MARKER
⊕	AEQNBK	EQUATION BACK	⊕	DST"X" P	STRUCTURE, RECT., TYPE "X" "X" = I, K, L, M, O, P, U	⊕	ICNTLCAB	CONTROLLER CABINET	⊕	MEPP.P	EASEMENT, PERM., BACK LINE	⊕	SRSC4	SHLD, CTY, 4 DIG.	⊕	UGP	GAS/FUEL PUMP
⊕	AEVT	EVENT STATION	ENVIRONMENTAL			⊕	ICPB	COMMUNICATION PULL BOX	⊕	MEPSP.P	EASEMENT, PERM., SHAPE	⊕	SRSC2	SHLD, CTY TOUR, 1-2 DIG.	⊕	UGV	GAS, VALVE
⊕	APC	POINT OF CURVATURE				⊕	ICTD	CONDUIT TURNING DOWN	⊕	MFAP.P	FEE ACQUISITION, APPROX.	⊕	SRSC4	SHLD, CTY TOUR, 3-4 DIG.	⊕	UGVT	GAS, VENT
⊕	APCC	POINT OF COMPOUND CURVATURE	⊕	ICVT	COMM. VEH. ROAD TRANSCEIVER	⊕	MFP.P	FEE ACQUISITION, BACK LINE	⊕	SRSI	SHLD, INTERSTATE	⊕	SRSI	SHLD, INTERSTATE	⊕	ULP	LIGHTING, POLE
△	API	POINT OF INTERSECTION	+	IDEFAULT	DEFAULT	⊕	MFSP.P	FEE ACQUISITION, SHAPE	⊕	SRSN2	SHLD, NATIONAL, 2 DIG.	⊕	SRSN2	SHLD, NATIONAL, 2 DIG.	⊕	ULPM	LIGHTING, POLE, MEDIAN
△	APOB	POINT OF BEGINNING	⊕	IEZR	E-ZPASS READER	⊕	MHBAP	HIGHWAY BNDRY., APPROX.	⊕	SRSN3	SHLD, NATIONAL, 3 DIG.	⊕	SRSN3	SHLD, NATIONAL, 3 DIG.	⊕	ULPP	LIGHTING, POLE, PED.
⊕	APOC	POINT OF CURVATURE	⊕	IEZTR	TRANSMITTAL READER	⊕	MHBCP	HISTORICAL, BLDG. CORNERS	⊕	SRSS2	SHLD, STATE, 2 DIG.	⊕	SRSS2	SHLD, STATE, 2 DIG.	⊕	UMFC	MISC. FILLER CAP
△	APOE	POINT OF END	⊕	IFOXCAB	FIBER OPTIC X-CONNECT CABINET	⊕	MHBP	HIGHWAY BNDRY, PT.	⊕	SRSS3	SHLD, STATE, 3 DIG.	⊕	SRSS3	SHLD, STATE, 3 DIG.	⊕	UOLM	OIL, LINE MARKER
⊕	APOL	POINT ON LINE	⊕	IFUSSPL	FUSION SPLICE	⊕	MJCP	PT., JURIS. CITY	⊕	SRSS4	SHLD, STATE, 4 DIG.	⊕	SRSS4	SHLD, STATE, 4 DIG.	⊕	UP	POLE, WITH UTILITY
⊕	APOS	POINT ON SPIRAL	⊕	IHARADV	HAR ADVISORY SIGN	⊕	MPBC	PT., BUILDING CORNER	TRAFFIC CONTROL			⊕	UPD	POLE, DEAD (NO UTILITY)			
⊕	APOT	POINT ON TANGENT	⊕	IHARST	HAR SITE	⊕	MPC	PT., CROSS CUT				⊕	UPD	POLE, WITH LIGHT			
△	APOVC	POINT ON VERTICAL CURVE	⊕	IILC	LOAD CENTER	⊕	MPDH	PT., DRILL HOLE	⊕	TCBJ	BOX, JUNCTION	⊕	USMH	SANITARY SEWER MANHOLE			
△	APOVT	POINT ON VERTICAL TANGENT	⊕	IMECSPL	MECHANICAL SPLICE	⊕	MPF	PT., FENCE LOCATION	⊕	TCBP	BOX, PULL BOX	⊕	UTB	TELEPHONE, BOOTH			
Y	APORC	POINT ON REVERSE CURVE	⊕	IMSCS	PORT. SPEED & COUNT SENSOR	⊕	MPIP	PT., IRON PIPE	⊕	TCBS	BOX, SPLICE	⊕	UTLM	TELEPHONE, LINE MARKER			
⊕	APT	POINT OF TANGENCY	⊕	IMSCS	MICRO SPEED & COUNT SENSOR	⊕	MPIR	PT., IRON ROD	⊕	TCMC	MICROCOMPUTER CABINET	⊕	UTMH	TELEPHONE, MANHOLE			
⊕	APVC	POINT OF VERTICAL CURVATURE	⊕	IMT	MICROWAVE TRANSCEIVER	⊕	MPM	PT., MONUMENT	⊕	TCPP	PED POLE	⊕	UTVLM	CABLE TV, LINE MARKER			
△	APVCC	POINT OF VERT. CMPND CURVE	+	IOVHMS	PERM. OVERHEAD VMS	⊕	MPMM	PT., MONUMENT, MISC.	⊕	TCSH	SIGNAL HEADS	⊕	UTVPB	CABLE TV, PULL BOX			
⊕	APVI	POINT OF VERT. INTERSECTION	⊕	IPASCS	PORT. ACCOU. SPD & CNT. SENSOR	⊕	MPN	PT., NAIL	TRAFFIC WORK ZONE			⊕	UUB	UNKNOWN, BOX			
△	APVRC	POINT OF VERT. REVERSE CURVE	⊕	IPEDS	PEDESTRIAN SIGNAL HEAD	⊕	MPRS	PT., RAILROAD SPIKE				⊕	UUB	UNKNOWN, BOX			
⊕	APVT	POINT OF VERTICAL TANGENCY	+	IPSS	PAVEMENT SURFACE SENSOR	⊕	MPST	PT., SPIKE	⊕	UUB	UNKNOWN, BOX	⊕	UUB	UNKNOWN, BOX			
⊕	ASC	SPIRAL TO CURVE	⊕	IPVMS	PERM. VMS	⊕	MPTW	PT., TREE W/ WIRE	⊕	UUB	UNKNOWN, BOX	⊕	UUB	UNKNOWN, BOX			
△	ASPI	SPIRAL POINT OF INTERSECTION	⊕	IRM	RAMP METER	⊕	MPWL	PT., WALL LOCATION	⊕	UUB	UNKNOWN, BOX	⊕	UUB	UNKNOWN, BOX			
⊕	ASTS	SPIRAL TO SPIRAL	⊕	IRWIS	RDWY WEATHER INFO. SENSOR	ROW ACQUISITION			⊕	UUB	UNKNOWN, BOX	⊕	UUB	UNKNOWN, BOX			
⊕	AST	SPIRAL TO TANGENT	⊕	IS	SOLAR PANEL				⊕	MFS.P.T	FEE ACQUISITION	⊕	UUB	UNKNOWN, BOX	⊕	UUB	UNKNOWN, BOX
⊕	ATS	TANGENT TO SPIRAL	⊕	ISST	SPREAD SPECT. TRANSCEIVER	⊕	MEPS.P.T	EASEMENT, PERMANENT	⊕	UUB	UNKNOWN, BOX	⊕	UUB	UNKNOWN, BOX			
△	AVEVT	VERTICAL EVENT POINT	⊕	ITDB	TELEPHONE DEMARCATION BLK	⊕	METS.P.T	EASEMENT, TEMPORARY	⊕	UUB	UNKNOWN, BOX	⊕	UUB	UNKNOWN, BOX			
⊕	AVHIGH	VERTICAL HIGH POINT	⊕	ITP	SUBSURFACE TEMP. PROBE	⊕	METS.P.T	OCCUPANCY, TEMPORARY	⊕	UUB	UNKNOWN, BOX	⊕	UUB	UNKNOWN, BOX			
⊕	AVLOW	VERTICAL LOW POINT	⊕	IVTRT	VEHICLE TO RDWY TRANSCEIVER	⊕	METS.P.T	FEE ACQUISITION W/O ACCESS	⊕	UUB	UNKNOWN, BOX	⊕	UUB	UNKNOWN, BOX			
BRIDGE			⊕	IWIMD	WEIGHT IN MOTION DETECTOR	ROADWAY			⊕	RES P	ELEVATION, SPOT	⊕	UUB	UNKNOWN, BOX			
			⊕	IWVR	WIRELESS VIDEO REPEATER				⊕	RGA	GUIDE RAIL, ANCHOR	⊕	UUB	UNKNOWN, BOX			
⊕	BSC	BRIDGE, SCUPPER	⊕	IWVRC	WIRELESS VIDEO RECEIVER	⊕	RGP	GUIDE POST, SINGLE	⊕	UUB	UNKNOWN, BOX	⊕	UUB	UNKNOWN, BOX			
CONTROL			+	IWVTT	WIRELESS VIDEO TRANSMITTER	ROADWAY			⊕	UUB	UNKNOWN, BOX	⊕	UUB	UNKNOWN, BOX			
			⊕	LWV	WIRELESS VIDEO RECEIVER				⊕	UUB	UNKNOWN, BOX	⊕	UUB	UNKNOWN, BOX			
△	CBP	BASELINE, POINT	⊕	LWV	WIRELESS VIDEO RECEIVER	ROADWAY			⊕	UUB	UNKNOWN, BOX	⊕	UUB	UNKNOWN, BOX			
⊕	CBPOL	BASELINE, POINT ON LINE	+	LUKP	UNKNOWN POINT				⊕	UUB	UNKNOWN, BOX	⊕	UUB	UNKNOWN, BOX	⊕	UUB	UNKNOWN, BOX
⊕	CBSP	BASELINE, SPUR POINT	1. THE LEGEND ILLUSTRATES MAPPING FEATURES (EXISTING AND PROPOSED). 2. FEATURES ARE SHOWN AS EITHER LINEAR (ROADWAY GUIDERAIL, ROADWAY SIDEWALK, UTILITY LINES, ETC.) OR POINT (SIGN, UTILITY POLE, ETC.). 3. FEATURES SHOWN ON THE LEGEND AS EXISTING FEATURES ALSO HAVE CORRESPONDING PROPOSED FEATURES. 4. PROPOSED FEATURE SYMBOLOGY IS IDENTICAL TO EXISTING FEATURE SYMBOLOGY EXCLUDING LINE WEIGHT. LINE WEIGHT FOR PROPOSED FEATURES IS THICKER (0.015 in ON B SIZE DRAWINGS). 5. MAPPING FEATURES NOT INCLUDED ON THE LEGEND SHEET DO NOT HAVE A UNIQUE SYMBOLOGY (SUCH AS THE PAVEMENT EDGE, PAVEMENT EDGE OF TRAVEL WAY) AND SHOULD BE LABELED ON THE PLANS. 6. FEATURES SHOWN AT THE HEAVIER WEIGHT ARE PROPOSED ONLY AND DO NOT HAVE CORRESPONDING EXISTING FEATURES.			⊕	UUB	UNKNOWN, BOX	⊕	UUB	UNKNOWN, BOX	⊕	UUB	UNKNOWN, BOX			
⊕	CBTP	BASELINE, TIE POINT				⊕	UUB	UNKNOWN, BOX	⊕	UUB	UNKNOWN, BOX	⊕	UUB	UNKNOWN, BOX	⊕	UUB	UNKNOWN, BOX
⊕	CPBM	BENCHMARK				⊕	UUB	UNKNOWN, BOX	⊕	UUB	UNKNOWN, BOX	⊕	UUB	UNKNOWN, BOX	⊕	UUB	UNKNOWN, BOX
⊕	CPH	POINT, HORIZ. PHOTOGRAMMETRY				⊕	UUB	UNKNOWN, BOX	⊕	UUB	UNKNOWN, BOX	⊕	UUB	UNKNOWN, BOX	⊕	UUB	UNKNOWN, BOX
⊕	CPSM	POINT, SURVEY MARKER, PERM.				⊕	UUB	UNKNOWN, BOX	⊕	UUB	UNKNOWN, BOX	⊕	UUB	UNKNOWN, BOX	⊕	UUB	UNKNOWN, BOX
⊕	CPSV	POINT, VERT., PHOTOGRAMMETRY				⊕	UUB	UNKNOWN, BOX	⊕	UUB	UNKNOWN, BOX	⊕	UUB	UNKNOWN, BOX	⊕	UUB	UNKNOWN, BOX

NY ROUTE 74 OVER EAGLE LAKE CHANNEL	PIN 1110.02	BRIDGES	1080770	CULVERTS	C120086	ALL DIMENSIONS IN FT UNLESS OTHERWISE NOTED	CONTRACT NUMBER	D264836
CULVERT REPLACEMENT	UTILITY QUALITY LEVEL "C"					LEGEND POINT SYMBOLOGY	DRAWING NO.	LEG-01
SH 8230 SCHROON LAKE - TICONDEROGA, PART 2							SHEET NO.	3
TOWN OF TICONDEROGA								
COUNTY: ESSEX COUNTY	REGION: 1							

FILE NAME = N:\Projects\2020\126-209 NYS DOT - Eagle Lake Workung\CADD\ dgn\111892_oph_1.leg.dgn
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 USER = KDetrick
 PLOT = NYS DOT.DWG.PDF.plt

JOB SUPERVISOR J. PANGBURN
 DESIGN L. SHORT
 CHECK A. BROWN
 DRAFTING K. DETRICK
 CHECK A. BROWN
 PROJECT MANAGER C. GEROUX

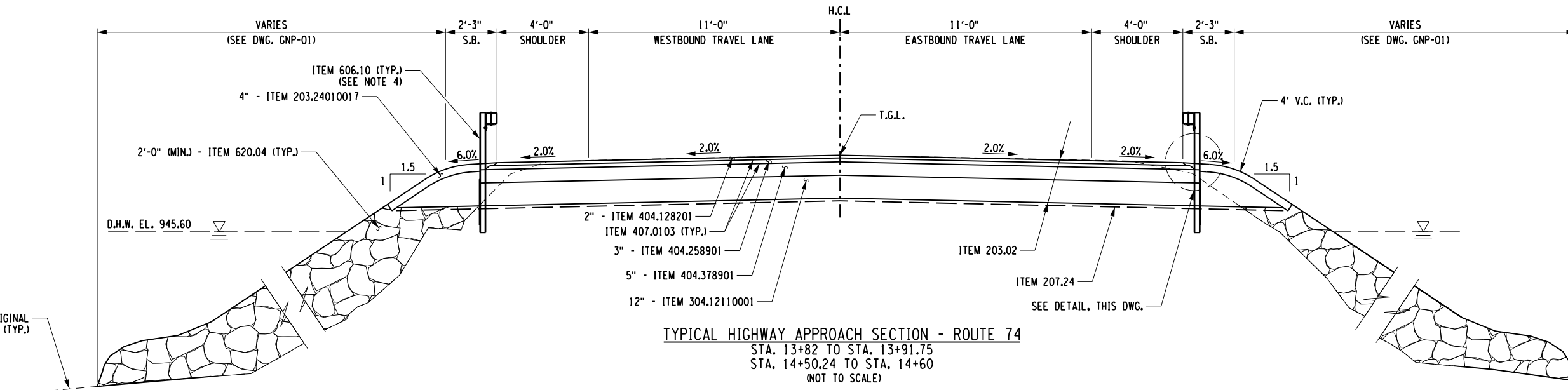
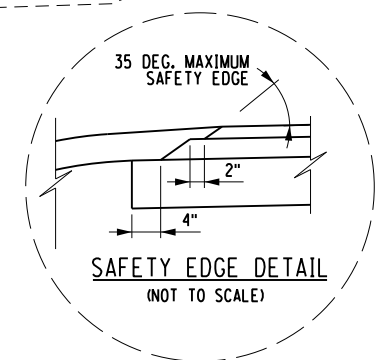
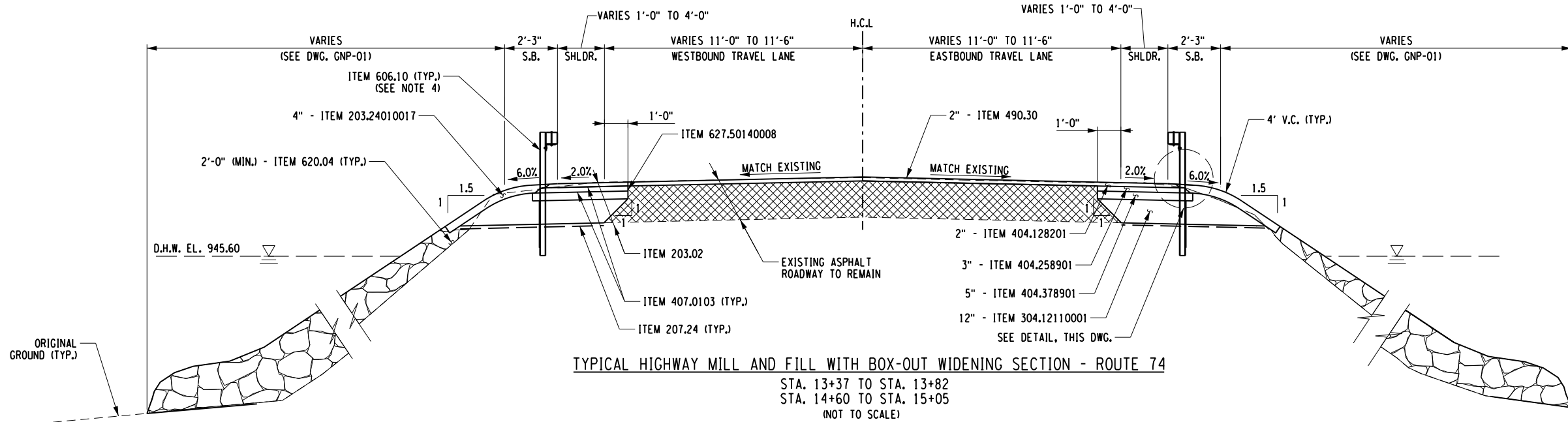
ALIGNMENT			LANDSCAPE			ROADWAY			TRAFFIC WORK ZONE		
STYLE	NAME	DESCRIPTION	STYLE	NAME	DESCRIPTION	STYLE	NAME	DESCRIPTION	STYLE	NAME	DESCRIPTION
	AC	CONTROL (CENTERLINE)		LABL	AREA, BRUSH LINE		RCZ_P	CLEAR ZONE		TWZBT_P	BARRIER, TEMPORARY
	AD_P	DETOUR		LAHR	AREA, HEDGE ROW		RG	GUIDE RAIL, MISCELLANEOUS		TWZBTWL_P	BARRIER, TEMPORARY, W/ WARNING LIGHTS
	AT_P	TRANSITION CONTROL		LAPB	AREA, PLANTING BED		RGB	GUIDE RAIL, BOX BEAM		TWZCD_P	CHANNELIZING DEVICE
BRIDGE				LAWA	AREA, WOODED AREA OUTLINE		RGBM	GUIDE RAIL, BOX BEAM, MEDIAN		TWZPMRC_P	PAVEMENT MARKING REMOVAL OR COVERING
	BR	RAIL		LAWC	AREA, WATERS EDGE		RGC	GUIDE RAIL, CABLE	UTILITIES		
	BSHT	SHEET PILING		LCUT_P	CUT LIMIT		RGCB	GUIDE RAIL, CONCRETE BARRIER	STYLE	NAME	DESCRIPTION
CONTROL				LFILL_P	FILL LIMIT		RGP_P	GUIDE POST		UC	CONDUIT, UNDERGROUND
	CB	BASELINE		LFNC	FENCE		RGW	GUIDE RAIL, W BEAM		UCH	CONDUIT, HANGING
	CBPR	BASELINE, PROJECTION		LTRC	TREE ROW, CONIFEROUS		RGWM	GUIDE RAIL, W BEAM, MEDIAN		UCO	CONDUIT, OVERHEAD
DRAINAGE				LTRD	TREE ROW, DECIDUOUS		RPB	PARKING BUMPER		UE	ELECTRIC LINE, UNDERGROUND
	DCP	CULVERT PIPE		LWH	WALL, H PILE		RRC	RAIL ROAD, CATENARY		UEH	ELECTRIC LINE, HANGING
	DCP_P	CULVERT PIPE (DIR)		LWR	WALL, RETAINING		RRER	RAIL ROAD, 3RD RAIL		UEO	ELECTRIC LINE, OVERHEAD
	DDG_P	DITCH, GRASS LINED		LWS	WALL, STONE		RRPLS_P	RAIL, PHOTO, LARGE SCALE		UETO	ELECTRIC TRANSMISSION, OVERHEAD
	DDP_P	DITCH, PAVED INVERT	ROW MAPPING				RRRER	RAIL, PHOTO, SMALL SCALE		UESS	ELECTRIC, SUBSTATIONS
	DDS_P	DITCH, STONE LINED		MDL	DEED LINE		RRPSS	RAIL, PHOTO, SMALL SCALE		UFO	FIBER OPTIC, UNDERGROUND
	DFL_P	FLOW LINE		MEE	EASEMENT, EXISTING		RRS	RUMBLE STRIP		UFOH	FIBER OPTIC, HANGING
	DSSD	SLOTTED DRAIN		MEP_P	EASEMENT, PERMANENT		RRSLS_P	RAIL, SURVEY, LARGE SCALE		UFOO	FIBER OPTIC, OVERHEAD
	DUD_P	UNDERDRAIN		MEPA_P	EASEMENT, PERMANENT, APPROX.		RRSSS	RAIL, SURVEY, SMALL SCALE		UG	GAS, UNDERGROUND
ENVIRONMENTAL				MET_P	EASEMENT, TEMPORARY	SIGNS				UGH	GAS, HANGING
	EBLHS	BALE, STRAW		META_P	EASEMENT, TEMPORARY, APPROX.		SBLB	BILLBOARDS		UGO	GAS, OVERHEAD
	ECT	CURTAIN, TURBIDITY		MF_P	FEE ACQUISITION, W/ ACCESS		SM	MULTIPLE POST		UIC	INFORM CABLE, UNDERGROUND
	EDMC	DAM, COFFER		MFA_P	FEE ACQUISITION, APPROXIMATE		SSO	STRUCTURE, OVERHEAD		UICH	INFORM CABLE, HANGING
	EDMEC_P	DAM, EARTHEN CHECK		MFS_P	FEE ACQUISITION, SHAPE		SSOC	STRUCTURE, OVHD. CANTILEVER		UO	OIL LINE, UNDERGROUND
	EDMGC_P	DAM, GRAVEL BAG/SAND BAG CHECK		MFWOA_P	FEE ACQUISITION, W/O ACCESS	STRIPING				UOH	OIL LINE, HANGING
	EDMPC_P	DAM, PREFABRICATED CHECK		MHA	HISTORICAL, ACQUISITION		STB*	BROKEN LINE		UPBP	POLE, BRACE, PUSH BRACE
	EDMSC_P	DAM, STONE CHECK		MHB	HIGHWAY BOUNDARY		STDB*	DOUBLE BROKEN LINE		UPGW	POLE, GUY WIRE
	EFNS	FENCE, SILT		MHBA	HIGHWAY BOUNDARY, APPROX.		STDL*	DOTTED LINE LONG		USA	SANITARY SEWER, UNDERGROUND
	EFNSV	FENCE, SILT & VEGETATION		MHBW	HWY BOUNDARY, FACE OF WALL		STDS*	DOTTED LINE SHORT		USAH	SANITARY SEWER, HANGING
	EFNV	FENCE, VEGETATION		MHBWOA	HIGHWAY BOUNDARY, W/O ACCESS		STFB*	FULL BARRIER LINE		USAF	SANITARY SEWER, FORCE MAIN, UGND
	EWAA_P	WETLAND, ADJACENT AREA		MJC	JURISDICTION, CITY		STFH*	HATCH LINE		USAFH	SANITARY SEWER, FORCE MAIN, HANG
	EWF	WETLAND, FEDERAL		MJCY	JURISDICTION, COUNTY		STH*	HATCH LINE		UT	TELEPHONE, UNDERGROUND
	EWFs	WETLAND, FEDERAL AND STATE		MJHD	JURISDICTION, HISTORIC DISTRICT		STPB*	PARTIAL BARRIER LINE		UTH	TELEPHONE, HANGING
	EWM	WETLAND, MITIGATION AREA		MJLL	JURIS., (GREAT, MILITARY) LOT LINE		STRCT	ROUNDABOUT, CAT TRACKS		UTO	TELEPHONE, OVERHEAD
	EWS	WETLAND, STATE		MJN	JURISDICTION, NATION		STRYL	ROUNDABOUT, YIELD LINE		UTV	CABLE TV, UNDERGROUND
				MJPB	JURISDICTION, PUBLIC LANDS		STSB	STOP BAR		UTVH	CABLE TV, HANGING
				MJS	JURISDICTION, STATE		STSE*	SOLID, EDGE		UTVO	CABLE TV, OVERHEAD
				MJT	JURISDICTION, TOWN		STXL	X WALK, LADDER LINE		UU	UNKNOWN, UNDERGROUND
				MJV	JURISDICTION, VILLAGE		STXLB	X WALK, LADDER BAR LINE		UUH	UNKNOWN, HANGING
				MPL	PROPERTY LOT LINE	* = W (WHITE) OR Y (YELLOW)				UUO	UNKNOWN, OVERHEAD
				MPLA	PROPERTY LOT LINE, APPROXIMATE	TRAFFIC CONTROL				UW	WATER LINE, UNDERGROUND
				MSL	SUB LOT LINE		TCSW	SIGNAL, SPAN WIRE		UWH	WATER LINE, HANGING
										UWO	WATER LINE, OVERHEAD

- THE LEGEND ILLUSTRATES MAPPING FEATURES (EXISTING AND PROPOSED).
- FEATURES ARE SHOWN AS EITHER LINEAR (ROADWAY GUIDERAIL, ROADWAY SIDEWALK, UTILITY LINES, ETC.) OR POINT (SIGN, UTILITY POLE, ETC.).
- FEATURES SHOWN ON THE LEGEND AS EXISTING FEATURES ALSO HAVE CORRESPONDING PROPOSED FEATURES.
- PROPOSED FEATURE SYMBOLOGY IS IDENTICAL TO EXISTING FEATURE SYMBOLOGY EXCLUDING LINE WEIGHT. LINE WEIGHT FOR PROPOSED FEATURES IS THICKER (0.015 in ON B SIZE DRAWINGS).
- MAPPING FEATURES NOT INCLUDED ON THE LEGEND SHEET DO NOT HAVE A UNIQUE SYMBOLOGY (SUCH AS THE PAVEMENT EDGE, PAVEMENT EDGE OF TRAVEL WAY) AND SHOULD BE LABELED ON THE PLANS.
- FEATURES SHOWN AT THE HEAVIER WEIGHT ARE PROPOSED ONLY AND DO NOT HAVE CORRESPONDING EXISTING FEATURES.

NY ROUTE 74 OVER EAGLE LAKE CHANNEL	PIN 1110.02	BRIDGES	CULVERTS	ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED	CONTRACT NUMBER
CULVERT REPLACEMENT	UTILITY QUALITY LEVEL "C"	1080770	C120086	LEGEND LINE SYMBOLOGY	D264836
SH 8230 SCHROON LAKE - TICONDEROGA, PART 2					DRAWING NO. LEG-02
TOWN OF TICONDEROGA					SHEET NO. 4
COUNTY: ESSEX COUNTY	REGION: 1				

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 USER = KDetrick
 PLOT = NYS DOT CME PDF.plt:fg

PROJECT MANAGER C. GEROUX
 CHECK A. BROWN
 DRAFTING K. DETRICK
 CHECK A. BROWN
 DESIGN L. SHORT
 JOB MANAGER C. GEROUX
 DESIGN SUPERVISOR J. PANGBURN



PREPARED BY: JEFFREY W. PANGBURN, P.E. ALTERED BY: ON:

ITEM	DESCRIPTION	UNIT	ITEM	DESCRIPTION	UNIT
203.02	UNCLASSIFIED EXCAVATION AND DISPOSAL	CY	407.0103	STRAIGHT TACK COAT	GAL
203.24010017	SHOULDER BACKUP MATERIAL	TON	418.7603	ASPHALT PAVEMENT JOINT ADHESIVE	LF
207.24	GEOTEXTILE STABILIZATION	SY	490.30	MISCELLANEOUS COLD MILLING OF BITUMINOUS CONCRETE	SY
304.12110001	SUBBASE COURSE, ADIRONDACK OPTIONAL	CY	606.10	BOX BEAM GUIDE RAILING	LF
404.128201	12.5 F2 TOP COURSE WMA, 80 SERIES COMPACTION	TON	620.04	STONE FILLING (MEDIUM)	CY
404.258901	25 F9 BINDER COURSE WMA, 80 SERIES COMPACTION	TON	627.50140008	CUTTING PAVEMENT	LF
404.378901	37.5 F9 BASE COURSE WMA, 80 SERIES COMPACTION	TON			

NOTES:

- FIELD CONDITIONS MAY REQUIRE FLATTER OR STEEPER SLOPES THAN THOSE SHOWN TO PROVIDE A SMOOTH TRANSITION BETWEEN PROPOSED AND EXISTING SIDE SLOPES A.O.B.E.
- ASPHALT JOINT ADHESIVE SHALL BE APPLIED TO ALL THE VERTICAL FACES OF ALL LONGITUDINAL, TRANSVERSE, SAWCUT, AND OTHER JOINTS BUTTING AGAINST THE NEW TOP COURSE, ITEM 418.7603.
- STRAIGHT TACK COAT SHALL BE APPLIED TO ALL MILLED, OVERLAYED, SAWCUT SURFACES AND BETWEEN ALL LAYERS OF NEW ASPHALT, ITEM 407.0103.
- SEE TABLE OF GUIDE RAIL DWG. MST-03, FOR GUIDE RAIL LOCATIONS.

AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:	NY ROUTE 74 OVER EAGLE LAKE CHANNEL CULVERT REPLACEMENT SH 8230 SCHROON LAKE - TICONDEROGA, PART 2 TOWN OF TICONDEROGA COUNTY: ESSEX COUNTY REGION: 1	PIN 1110.02 UTILITY QUALITY LEVEL "C"	BRIDGES 1080770	CULVERTS C120086	ALL DIMENSIONS IN FT UNLESS OTHERWISE NOTED	CONTRACT NUMBER D264836
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TYPICAL SECTIONS

DRAWING NO. TYP-01
SHEET NO. 5

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 DATE/TIME = 4/27/2022 2:28:42 PM
 USER = KDetrick
 PLOT = NYS DOT CME PDF.plt:cfg

DESIGN SUPERVISOR J. PANGBURN
 DESIGN L. SHORT
 CHECK A. BROWN
 DRAFTING K. DETRICK
 CHECK A. BROWN
 PROJECT MANAGER C. GEROUX

WORK ZONE TRAFFIC CONTROL NOTES:

1. THE WORK ZONE TRAFFIC CONTROL (WZTC) PLANS IN NYS DOT STANDARD SHEETS ARE STANDARD FOR MOST COMMON SITUATIONS. THE CONTRACTOR SHALL PROVIDE ADDITIONAL PROTECTION PAID UNDER ITEM 619.01 AS DIRECTED BY THE ENGINEER WHERE SPECIAL CIRCUMSTANCES OCCUR.
2. ALL TRAFFIC CONTROL DEVICES USED FOR WORK ZONE TRAFFIC CONTROL SHALL CONFORM TO THE APPLICABLE SPECIFICATIONS AS SET FORTH IN THE MUTCD 2009 EDITION WITH REVISIONS 1 AND 2, THE NYS SUPPLEMENT AND THE MOST CURRENT NYS DOT STANDARD SPECIFICATIONS.
3. DRIVING AGAINST TRAFFIC AT ANY TIME, REGARDLESS OF WHETHER THE AREA HAS BEEN CLOSED TO TRAFFIC, SHALL NOT BE PERMITTED, EXCEPT FOR TRAFFIC CONE PICKUP, AND AS SPECIFICALLY PERMITTED BY THE ENGINEER.
4. WHEN REOPENING DRIVING LANES TO TRAFFIC, THE CONTRACTOR SHALL START BY REMOVING THE CONES AT THE FAR END OF THE LANE CLOSURE AND WORKING TOWARDS THE SIGNS AT THE BEGINNING OF THE LANE CLOSURE. SIGNS ARE NOT TO BE TAKEN DOWN UNTIL ALL TRAFFIC CONTROL DEVICES HAVE BEEN REMOVED.
5. PRIOR TO ANY REDUCTION IN ROADWAY WIDTH, THE CONTRACTOR SHALL PROVIDE THE ENGINEER TWENTY ONE (21) DAYS NOTICE IN WRITING SO HE/SHE MAY CONTACT THE REGIONAL PERMIT ENGINEER OF THE WIDTH RESTRICTION IN A TIMELY MANNER. THE CONTRACTOR SHALL NOTIFY THE ENGINEER IN WRITING IMMEDIATELY UPON THE REMOVAL OF THE LANE WIDTH RESTRICTION SO THE ENGINEER MAY NOTIFY THE REGIONAL PERMIT ENGINEER.
6. EXCAVATIONS THAT PRODUCE DROP-OFFS ON BOTH SIDES OF THE TRAVEL WAY AT THE SAME TIME SHALL NOT BE PERMITTED. SHOULDER AREAS SHOULD BE PREPARED TO RECEIVE THE SHOULDER PAVEMENT MATERIAL IMMEDIATELY AHEAD OF THE SHOULDER PAVING OPERATIONS TO MINIMIZE THE TIME A DROP-OFF EXISTS. "NO SHOULDER" (W08-23) SIGNS SHALL BE ERECTED NO MORE THAN 1,300 FT APART THROUGHOUT THE PROJECT WHERE A DROP-OFF EXISTS.
7. THE CONTRACTOR SHALL PROVIDE PAVEMENT MARKINGS FOR TRAFFIC CONTROL IN CONFORMANCE WITH THE REQUIREMENTS OF TEMPORARY AND INTERIM PAVEMENT MARKINGS. IT MAY BE NECESSARY TO MARK, REMARK, ALTER AND/OR OBLITERATE PAVEMENT MARKINGS WHERE DIRECTED BY THE ENGINEER. THE MARKINGS SHALL BE REMOVED BY MEANS WHICH COMPLETELY REMOVE ALL TRACES OF STRIPES. ALL TOP COURSE STRIPING SHALL CONFORM TO THE PROJECT'S FINAL MARKINGS WITH RESPECT TO COLOR AND PATTERN.

8. "IN LANE" SIGNS (NYW5-32P) AND BICYCLE SIGN (W11-1) SHALL BE INSTALLED, AS DIRECTED BY THE ENGINEER, WHEN CONSTRUCTION REQUIRES BICYCLE TRAFFIC TO USE THE TRAVEL LANE.
9. ALL TYPE III BARRICADES AT CLOSURE POINTS SHALL HAVE WARNING LIGHTS.
10. IF THE ENGINEER NOTIFIES THE CONTRACTOR OR HIS SUPERINTENDANT OF ANY HAZARDOUS CONSTRUCTION PRACTICES, ALL OPERATIONS IN THAT AREA SHALL BE DISCONTINUED AND IMMEDIATE REMEDIAL ACTION SHALL BE TAKEN TO THE SATISFACTION OF THE ENGINEER BEFORE WORK IS RESUMED.
11. THIS CONTRACT INCLUDES THE USE OF PORTABLE VARIABLE MESSAGE DISPLAY UNITS, ITEM 619.111113. THE CONTRACTOR SHALL UTILIZE PVMS TO WARN OF PROPOSED LANE CLOSURE SCHEDULES FOR ALL NIGHT WORK. THE PVMS SHALL BE PLACED NO LESS THAN THREE DAYS PRIOR TO THE START OF LANE CLOSURES AND REMAIN UP THROUGHOUT THE DURATION OF LANE CLOSURES.
12. IF, IN THE ENGINEER'S JUDGEMENT, FLAGS ON SIGNS ARE NECESSARY DUE TO LIMITED SIGHT DISTANCE, THEY SHALL BE PROVIDED BY THE CONTRACTOR. COST SHALL BE INCLUDED IN ITEM 619.01 - BASIC WORK ZONE TRAFFIC CONTROL.
13. CONSTRUCTION VEHICLES, EQUIPMENT AND MATERIALS:
 - A. THE CLEAR ROADSIDE AREA IS DEFINED AS THE AREA WITHIN 10 FEET OF THE EDGE OF THE TRAVEL WAY.
 - B. CONTRACTOR VEHICLES NOT IN USE AND PRIVATE VEHICLES OWNED BY CONTRACTOR'S EMPLOYEES SHALL NOT BE PARKED IN THE CLEAR ROADSIDE AREA, OR ANY OTHER LOCATION CONSIDERED BY THE ENGINEER TO BE A HAZARD. THIS REQUIREMENT IS NOT LIMITED TO THE CONTRACT LIMITS.
 - C. NO MATERIAL IS TO BE STORED WITHIN THE CLEAR ROADSIDE AREA WITHOUT THE APPROVAL OF THE ENGINEER.
 - D. VEHICLES BELONGING TO THE CONTRACTOR OR THE CONTRACTOR'S EMPLOYEES SHALL NOT BE PARKED IN A MANNER WHICH OBSTRUCTS SIGNS, BARRIERS, BARRICADES OR OTHER TRAFFIC CONTROL DEVICES.
 - E. VEHICLES BELONGING TO THE CONTRACTOR OR THE CONTRACTOR'S EMPLOYEES SHALL NOT BE PARKED IN A MANNER WHICH INTERFERES WITH ACCESS TO ADJUTING PROPERTIES.
 - F. THE CONTRACTOR SHALL PLAN AND INCORPORATE ACCESS POINTS INTO THE WORK ZONE SUCH THAT, TO THE EXTENT PRACTICAL, THE CONTRACTOR'S VEHICLES ENTERING AND LEAVING THE WORK ZONE SHALL NOT IMPEDE THE MOVEMENT OF THROUGH TRAFFIC IN THE ADJACENT OPEN LANES.
14. THE CONTRACTOR SHALL NOTIFY THE ENGINEER AT LEAST 7 DAYS PRIOR TO PLACING THE TOP COURSE PAVEMENT IN ORDER TO CONTACT THE REGIONAL TRAFFIC ENGINEER TO SCHEDULE BALL BANKING TO DETERMINE RECOMMENDED SPEED. THE RESULTANT RECOMMENDED SPEEDS MAY JUSTIFY ADDING, MOVING OR ELIMINATING SOME OF THE CURVE WARNING SIGNS AND W13-1 ADVISORY SPEED SIGNS.
15. TRAFFIC SHALL BE ALLOWED TO BE MAINTAINED ON A MILLED SURFACE FOR A MAXIMUM OF SEVEN (7) CONSECUTIVE DAYS. DURING THIS PERIOD, ON ROADS WHERE THE POSTED SPEED LIMIT IS 45 MPH OR HIGHER, PVMS SHALL BE USED TO WARN OF THE CONDITION WITH THE FOLLOWING MESSAGES:

"MILLED SURFACE AHEAD"	"M CYCLES USE CAUTION"
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16. THE CONTRACTOR SHALL MAINTAIN PEDESTRIAN AND BICYCLE TRAFFIC THROUGH AND AROUND THE PROJECT FOR THE DURATION OF CONSTRUCTION. MATERIAL, EQUIPMENT OR OTHER BARRIERS SHALL NOT BE PLACED OR MARKED IN SUCH A MANNER AS TO OBSTRUCT PEDESTRIAN OR BICYCLE TRAFFIC OR TO PRESENT A SAFETY HAZARD TO THE NON-MOTORIZED PUBLIC. WHERE PEDESTRIAN TRAFFIC MUST BE RELOCATED OFF THE EXISTING FACILITY, WALKWAYS SHALL BE CLEARLY MARKED AND HAVE A LOGICAL START AND TERMINUS.
17. LANE CLOSURES AND/OR USE OF TEMPORARY CONCRETE BARRIER WITHIN FIVE FEET OF AN ACTIVE TRAVEL WAY BETWEEN NOVEMBER 15 & APRIL 15 SHALL REQUIRE SPECIAL PERMISSION FROM THE ENGINEER WITH CONCURRENCE FROM THE REGIONAL TRAFFIC OFFICE. THE CONTRACTOR SHALL NOTIFY THE ENGINEER ONE WEEK IN ADVANCE OF ANY REQUESTED CLOSURE.
18. ALL EXISTING TRAFFIC SIGNS THAT ARE APPROPRIATE SHALL REMAIN IN PLACE UNTIL THE NEW TRAFFIC SIGNS ARE INSTALLED.
19. AFTER PAVEMENT MARKINGS ARE REMOVED TEMPORARY EDGE LINES SHALL BE INSTALLED PRIOR TO OPENING TO TRAFFIC UNLESS THE SHOULDERS ARE OTHERWISE DELINEATED TO THE SATISFACTION OF THE ENGINEER. THE COST OF SUCH EDGE LINES OR DELINEATION IS TO BE INCLUDED IN THE PRICE BID FOR ITEM 619.01.
20. ALL INTERIM STRIPING IS TO BE REMOVED FROM THE TRAVEL LANES AND SHOULDERS AFTER THE INSTALLATION OF PERMANENT STRIPING PRIOR TO OPENING THE TRAVEL LANES TO TRAFFIC.
21. THE CONTRACTOR SHALL LIMIT LOUD AND SUSTAINED PROJECT NOISE AS DETERMINED BY THE ENGINEER TO THE EXTENT POSSIBLE BETWEEN 10:00 PM AND 6:00 AM.
22. THE CONTRACTOR SHALL EMPLOY A TRAFFIC CONTROL SUPERVISOR AS SPECIFIED IN SECTION 619-3.20 OF THE NYS DOT STANDARD SPECIFICATIONS. PAYMENT SHALL BE MADE UNDER ITEM 619.25, TRAFFIC CONTROL SUPERVISOR. THE TRAFFIC CONTROL SUPERVISOR SHALL REVIEW THE WORK ZONE TRAFFIC CONTROL PERIODICALLY AND PROVIDE THE ENGINEER WITH A WEEKLY LOG DOCUMENTING THE REVIEWS.
23. THE UNEVEN TRANSVERSE PAVEMENT SURFACES, (IE. PAVEMENT REBATES, END OF PAVED OR MILLED SECTIONS) SHALL BE POSTED WITH "BUMP" (W8-1) SIGNS TO BE LOCATED IN ADVANCE OF THE CONDITION, THE "BUMP" (W8-1) SIGNS SHALL BE PLACED ON ITS OWN POST. ALL PAVEMENT TRANSITIONS SHALL HAVE NO STEEPER THAN 1 ON 10 LONGITUDINAL SLOPE AS MEASURED IN THE DIRECTION OF TRAVEL.
24. THE CONTRACTOR SHALL INSTALL NON-INTRUSIVE DETECTION TO TRAFFIC SIGNALS AS OUTLINED IN THE PLANS BEFORE DISABLING THE EXISTING DETECTION SYSTEM EXCEPT WHEN DISABLING OF THE EXISTING DETECTION SYSTEM IS APPROVED BY THE ENGINEER.
25. TRAFFIC SHALL BE MAINTAINED ON A PAVED SURFACE AT ALL TIMES EXCEPT THAT, BETWEEN APRIL 15 AND NOVEMBER 15, TRAFFIC MAY BE MAINTAINED ON ITEM 304.12 FOR A MAXIMUM TEN (10) DAYS. FAILURE TO PROVIDE A PAVED SURFACE IN THE STATED TIME FRAME WILL RESULT IN THE ASSESSMENT OF LIQUIDATED DAMAGES IN THE AMOUNT OF \$1500 PER CALENDAR DAY.
26. A TAXI SERVICE SHALL BE PROVIDED AT LOCATIONS NOTED IN THE PLANS. TAXI AVAILABILITY SHALL BE WITHIN 30 MINUTES OF BEING REQUESTED. EXACT PICKUP LOCATION FOR PEDESTRIAN TRANSPORTATION SERVICE SHALL BE DETERMINED IN THE FIELD A.O.B.E.

SUGGESTED CONSTRUCTION SEQUENCING

- A. INSTALL ADVANCED WORK ZONE SIGNAGE ON NY ROUTE 74.
- B. INSTALL ADVANCED WORK ZONE SIGNAGE AND WORK ZONE TRAFFIC CONTROL DEVICES FOR EB LANE CLOSURE TO INSTALL MICROPILES WITHIN EB TRAVEL LANE. ONCE EB TRAVEL LANE IS COMPLETE, FLIP WORK ZONE PACKAGE FOR WB LANE CLOSURE TO INSTALL MICROPILES WITHIN WB TRAVEL LANE. REFER TO DWG. NO. ST-16 NOS. 1 THRU 5 FOR ADDITIONAL DETAILS. ONCE WB TRAVEL LANE IS COMPLETE, REMOVE WORK ZONE PACKAGE. MICROPILES SHALL BE INSTALLED IN THE FALL OF 2022.
- C. UTILIZING ALTERNATING ONE-WAY TRAFFIC W/ FLAGGING OPERATIONS REMOVE EXISTING GUIDE RAIL AND BOX-OUT WIDEN SHOULDERS TO BINDER COURSE.
- D. INSTALL DETOUR SIGNAGE AND WORK ZONE TRAFFIC CONTROL DEVICES FOR FULL ROADWAY CLOSURE. REMOVE EXISTING CULVERT AND INSTALL PROPOSED BRIDGE (REFER TO DWG. NO. ST-16 NOS. 6 THRU 15 FOR ADDITIONAL DETAILS). ONCE NEW STRUCTURE IS INSTALLED DETOUR TO BE REMOVED AND ROADWAY RE-OPENED TO TRAFFIC. CLOSURE W/ DETOUR SHALL OCCUR PRIOR TO MEMORIAL DAY WEEKEND 2023.
- E. UTILIZING ALTERNATING ONE-WAY TRAFFIC W/ FLAGGING OPERATIONS MILL EXISTING PAVEMENT. INSTALL APPROACH PAVEMENT TO BINDER COURSE, INSTALL GUIDE RAILING AND TOP COURSE PAVING W/ FINAL PAVEMENT MARKINGS AND SIGNAGE.
- F. CLEAN UP SITE, REMOVE ALL WORK ZONE TRAFFIC CONTROL SIGNAGE AND DEMOBILIZE.

WORK ZONE TRAFFIC CONTROL NOTES:

- A. THE WORK ZONE TRAFFIC CONTROL DETOUR PLAN AS SHOWN ON DWG. NO. WZP-05 IS SUMMARIZED AS FOLLOWS:
 1. TRAFFIC ON I-87 FROM THE SOUTH SHALL FOLLOW NY ROUTES 8 AND 9N TO NY ROUTE 74.
 2. TRAFFIC ON I-87 FROM THE NORTH SHALL FOLLOW NY ROUTES 9N AND 22 TO NY ROUTE 74.
 3. TRAFFIC ON NY ROUTE 74 FROM THE EAST SHALL FOLLOW NY ROUTES 9N AND 8 TO I-87 AT EXIT 25.
 4. TRAFFIC ON NY ROUTE 9N FROM THE SOUTH SHALL CONTINUE ON NY ROUTES 9N AND 22 TO I-87 AT EXIT 31.
 5. TRAFFIC ON NY ROUTE 9N FROM THE NORTH SHALL CONTINUE AND FOLLOW NY ROUTES 9N AND 8 TO I-87 AT EXIT 25.
 6. TRAFFIC ON NY ROUTE 9 FROM THE SOUTH SHALL FOLLOW NY ROUTES 8 AND 9N TO NY ROUTE 74.

PREPARED BY: JEFFREY W. PANGBURN, P.E. ALTERED BY: ON:



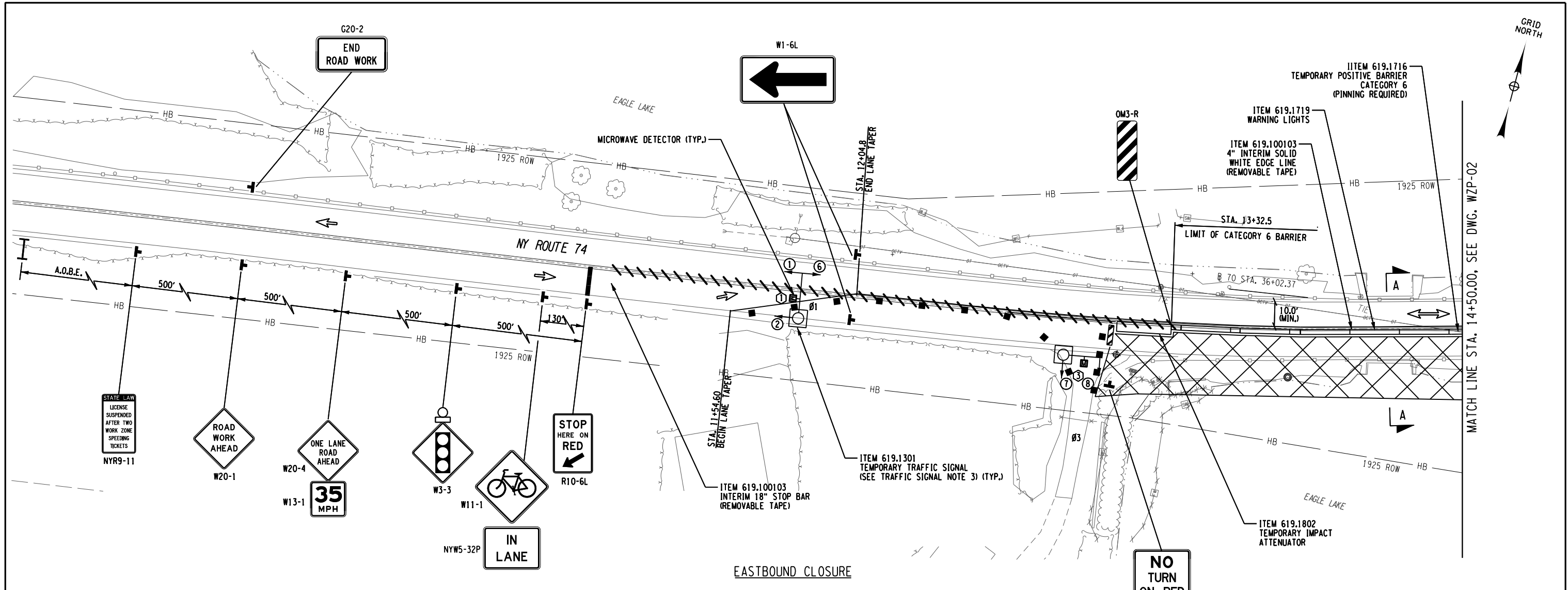
AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:	NY ROUTE 74 OVER EAGLE LAKE CHANNEL	PIN 1110.02 UTILITY QUALITY LEVEL "C"	BRIDGES 1080770	CULVERTS C120086	ALL DIMENSIONS IN FT UNLESS OTHERWISE NOTED	CONTRACT NUMBER D264836
	CULVERT REPLACEMENT					
	SH 8230 SCHROON LAKE - TICONDEROGA, PART 2					
	TOWN OF TICONDEROGA					
	COUNTY: ESSEX COUNTY REGION: 1				WORK ZONE TRAFFIC CONTROL NOTES	DRAWING NO. WZN-01 SHEET NO. 6

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 USER: KDetrick
 PLOT: NYSOT.DWG.PDF.plt:fg

PROJECT MANAGER: C. GEROUX
 CHECK: A. BROWN
 DRAFTING: K. DETRICK
 CHECK: A. BROWN
 DESIGN: L. SHORT
 JOB MANAGER: C. GEROUX
 DESIGN SUPERVISOR: J. PANGBURN

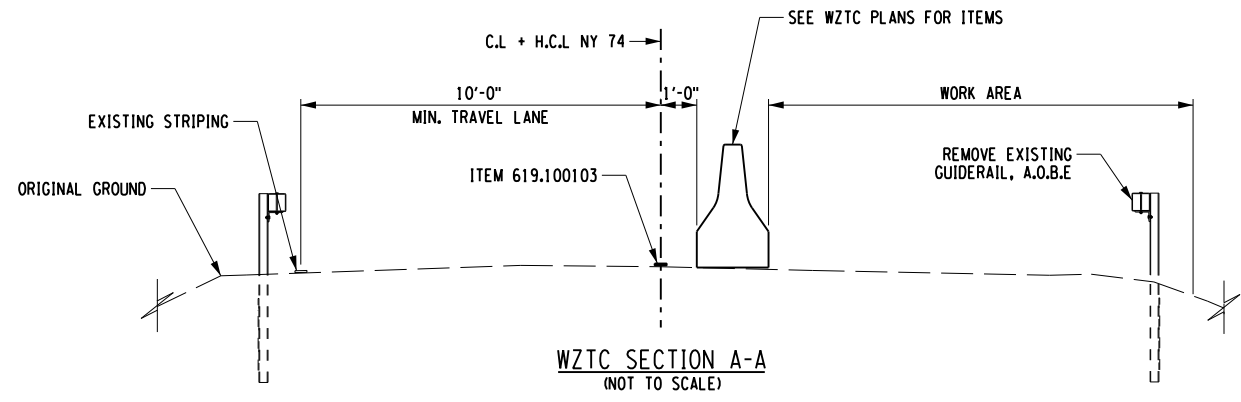


TRAFFIC SIGNAL NOTES:

1. DRIVEWAY EGRESS WILL BE CONTROLLED BY THE SIGNAL SYSTEM.
2. THE CONTRACTOR SHALL POST A CONSTRUCTION SIGN AT LEAST 14 DAYS PRIOR TO CLOSING ONE LANE OF TRAFFIC INDICATING THE DATE THAT THE LANE WILL CLOSE.
3. THE "EACH LOCATION" PAY UNIT FOR THE TEMPORARY SIGNALS REFERS TO EACH COMPLETE SIGNAL SYSTEM, EQUIPMENT FOR BOTH APPROACHES AND DRIVEWAYS ON THIS PROJECT ARE INCLUDED IN THE BID QUANTITY OF "1 EACH LOCATION."
4. TO RECONSTRUCT APPROACH LANES, CONTRACTOR SHALL COVER SIGNAL HEADS AND UNNEEDED SIGNAGE AND USE FLAGGER CONTROL TO ALTERNATE WORK ZONE TRAFFIC.

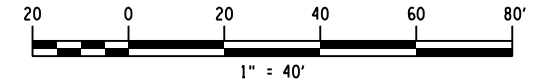
NOTES:

1. CONTRACTOR SHALL COVER ALL EXISTING STRIPING IN CONFLICT WITH WORK ZONE STRIPING, AND SHALL BE PAID FOR UNDER ITEM 619.0803. REMOVAL OF INTERIM PAVEMENT MARKING AND COVERED PAVEMENT MARKINGS BETWEEN WORK ZONE STAGES SHALL BE PAID FOR UNDER ITEM 619.080106.
2. EXISTING BOX BEAM GUIDE RAILING TO BE REMOVED AND RESET TO ALLOW FOR PILE OPERATIONS SHALL BE PAID FOR UNDER ITEM 606.53 (A.O.B.E.).



←	SIGNAL HEAD
■	MICROWAVE DETECTOR
⊙	DETECTOR NUMBER
⊘	PHASE NUMBER

+	WORK ZONE SIGN LOCATION
▨	TYPE III BARRICADE
■	CHANNELIZING DEVICE
▨▨▨	PAVEMENT MARKING COVER ITEM 619.0803
→	DIRECTION OF TRAVEL
T	PORTABLE VARIABLE MESSAGE SIGN ITEM 619.111113
▨▨▨	WORK ZONE



PREPARED BY: JEFFREY W. PANGBURN, P.E. ALTERED BY: ON:



AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:	NY ROUTE 74 OVER EAGLE LAKE CHANNEL	PIN 1110.02	BRIDGES	CULVERTS	ALL DIMENSIONS IN FT UNLESS OTHERWISE NOTED	CONTRACT NUMBER D264836
	CULVERT REPLACEMENT	UTILITY QUALITY LEVEL "C"	1080770	C120086		
	SH 8230 SCHROON LAKE - TICONDEROGA, PART 2					
	TOWN OF TICONDEROGA	REGION: 1				
					WORK ZONE TRAFFIC CONTROL PLAN EASTBOUND CLOSURE	DRAWING NO. WZP-01 SHEET NO. 8

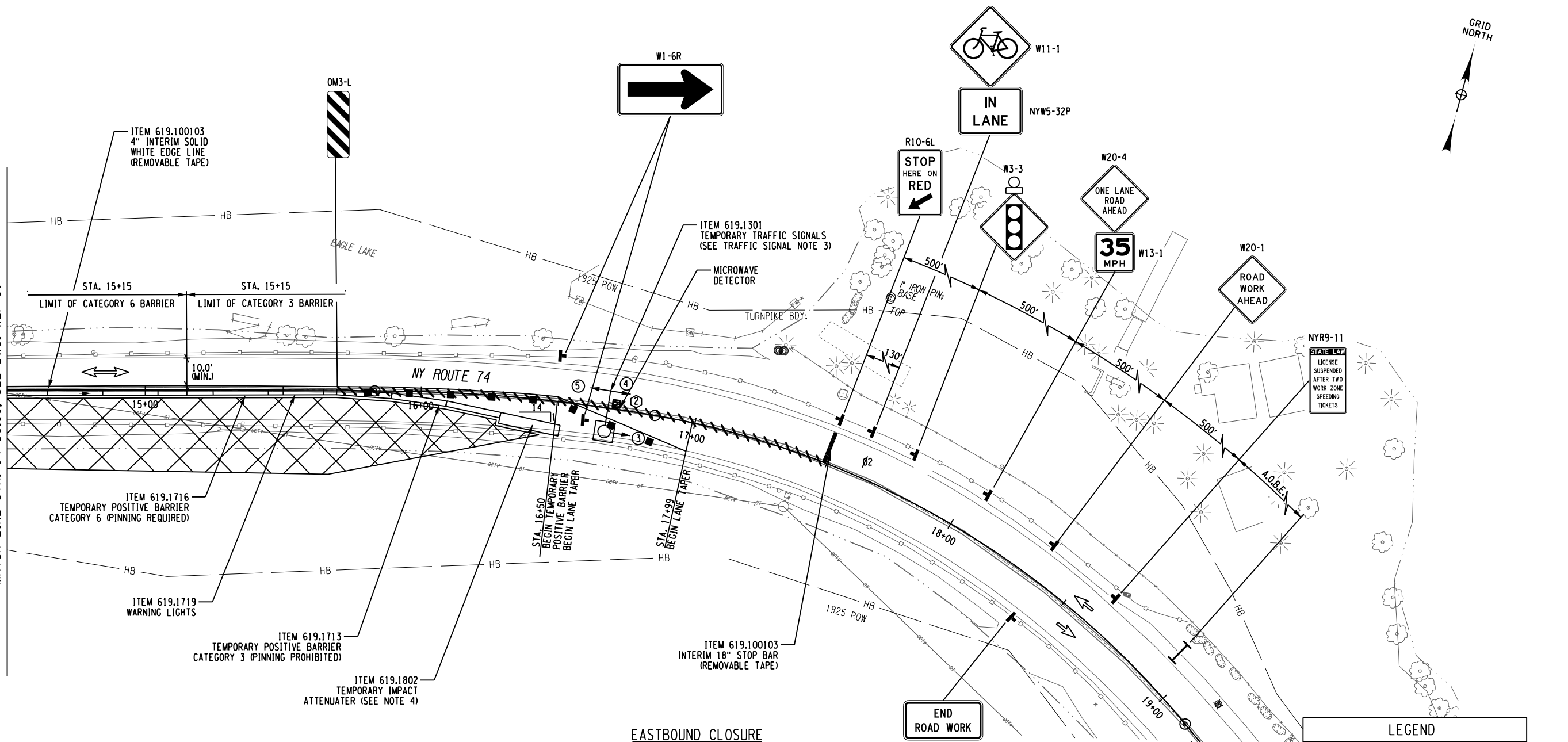
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.



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 PLOT = NYS DOT CME PDF.plt:fg

DESIGN SUPERVISOR: J. PANGBURN
 JOB MANAGER: C. GEROUX
 DESIGN: L. SHORT
 DRAFTING: K. DETRICK
 CHECK: A. BROWN
 PROJECT MANAGER: C. GEROUX

MATCH LINE STA. 14+50.00, SEE DWG. WZP-01



EASTBOUND CLOSURE

NOTES:

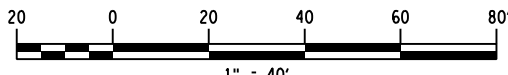
- CONTRACTOR SHALL COVER ALL EXISTING STRIPING IN CONFLICT WITH WORK ZONE STRIPING, AND SHALL BE PAID FOR UNDER ITEM 619.0803. REMOVAL OF INTERIM PAVEMENT MARKING AND COVERED PAVEMENT MARKINGS BETWEEN WORK ZONE STAGES SHALL BE PAID FOR UNDER ITEM 619.080106.
- EXISTING BOX BEAM GUIDE RAILING TO BE REMOVED AND RESET TO ALLOW FOR PILE OPERATIONS SHALL BE PAID FOR UNDER ITEM 606.53 (A.O.B.E.).
- REFER TO DWG. NO. WZP-01 FOR WORK ZONE STAGING TYPICAL SECTION A-A.
- WHEN TERMINATING THE APPROACH END OF TEMPORARY POSITIVE BARRIER (TPB), A TEMPORARY IMPACT ATTENUATOR SHALL BE USED WHEN THE BLUNT END OF THE TPB IS LESS THAN 12'-0" FROM THE WZTC EDGE OF TRAVELED WAY.

TRAFFIC SIGNAL NOTES:

- DRIVEWAY EGRESS WILL BE CONTROLLED BY THE SIGNAL SYSTEM.
- THE CONTRACTOR SHALL POST A CONSTRUCTION SIGN AT LEAST 14 DAYS PRIOR TO CLOSING ONE LANE OF TRAFFIC INDICATING THE DATE THAT THE LANE WILL CLOSE.
- THE "EACH LOCATION" PAY UNIT FOR THE TEMPORARY SIGNALS REFERS TO EACH COMPLETE SIGNAL SYSTEM, EQUIPMENT FOR BOTH APPROACHES AND DRIVEWAYS ON THIS PROJECT ARE INCLUDED IN THE BID QUANTITY OF "1 EACH LOCATION."
- TO RECONSTRUCT APPROACH LANES, CONTRACTOR SHALL COVER SIGNAL HEADS AND UNNEEDED SIGNAGE AND USE FLAGGER CONTROL TO ALTERNATE WORK ZONE TRAFFIC.

	SIGNAL HEAD
	MICROWAVE DETECTOR
	DETECTOR NUMBER
	PHASE NUMBER

	WORK ZONE SIGN LOCATION
	CHANNELIZING DEVICE
	PAVEMENT MARKING COVER ITEM 619.0803
	DIRECTION OF TRAVEL
	PORTABLE VARIABLE MESSAGE SIGN ITEM 619.111113
	WORK ZONE



PREPARED BY: JEFFREY W. PANGBURN, P.E. ALTERED BY: ON:

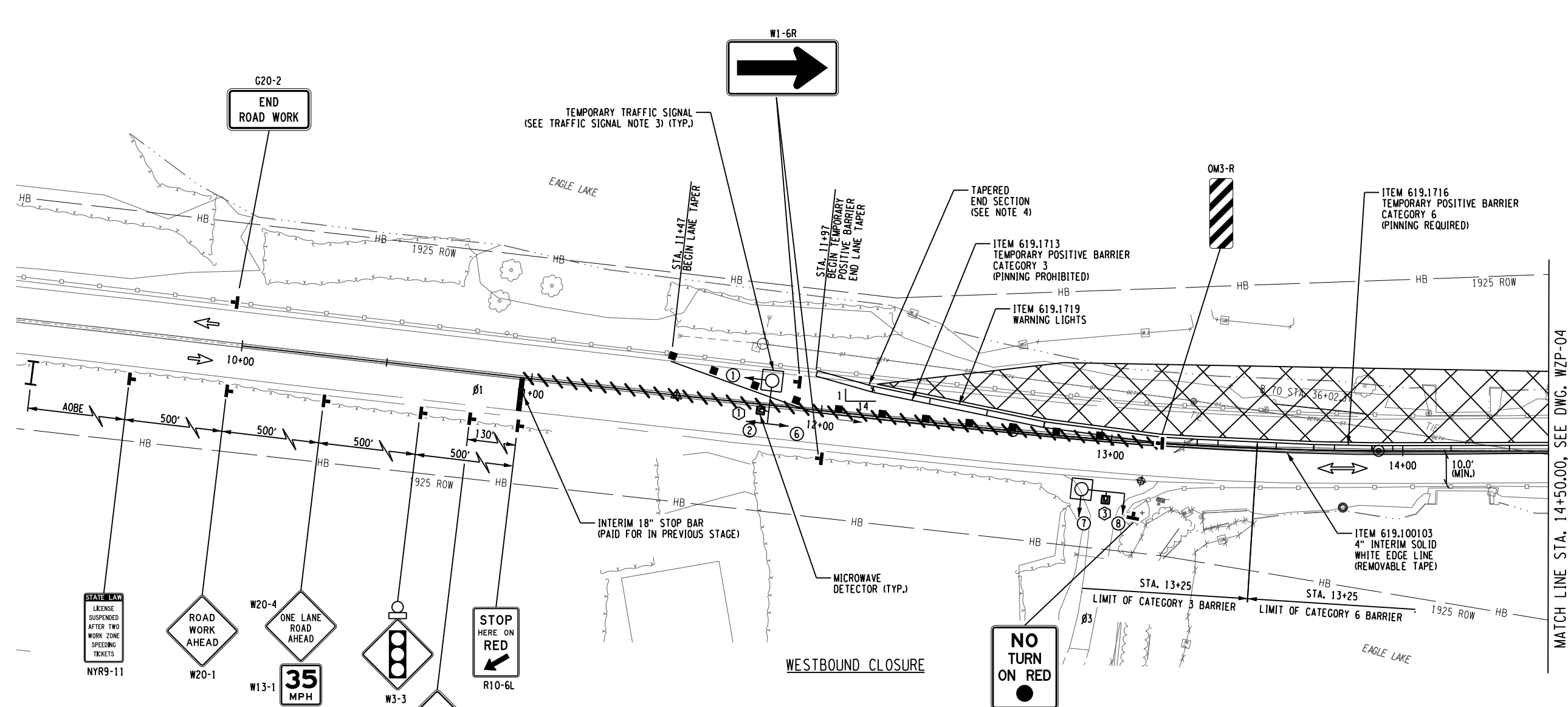


AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:	NY ROUTE 74 OVER EAGLE LAKE CHANNEL	PIN 1110.02	BRIDGES 1080770	CULVERTS C120086	ALL DIMENSIONS IN FT UNLESS OTHERWISE NOTED	CONTRACT NUMBER D264836
	CULVERT REPLACEMENT	UTILITY QUALITY LEVEL "C"			WORK ZONE TRAFFIC CONTROL PLAN EASTBOUND CLOSURE	DRAWING NO. WZP-02
	SH 8230 SCHROON LAKE - TICONDEROGA, PART 2					SHEET NO. 9
	TOWN OF TICONDEROGA					
	COUNTY: ESSEX COUNTY	REGION: 1				

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

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 PLOT = NYS DOT CME PDF.plt:fg

DESIGN SUPERVISOR J. PANGBURN
 JOB MANAGER C. GEROUX
 DESIGN L. SHORT
 CHECK A. BROWN
 DRAFTING K. DETRICK
 CHECK A. BROWN
 PROJECT MANAGER C. GEROUX



- TRAFFIC SIGNAL NOTES:**
1. DRIVEWAY EGRESS WILL BE CONTROLLED BY THE SIGNAL SYSTEM.
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 3. IF TRAILER-TYPE SIGNALS ARE UTILIZED, AND THE POSITIONS NEED TO BE ADJUSTED BETWEEN STAGES, CONTRACTOR SHALL INCLUDE COST OF THIS ADJUSTMENT IN THE UNIT PRICE BID FOR THE TEMPORARY TRAFFIC SIGNAL SYSTEM.
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- NOTES:**
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 2. EXISTING BOX BEAM GUIDE RAILING TO BE REMOVED AND RESET TO ALLOW FOR PILE OPERATIONS SHALL BE PAID FOR UNDER ITEM 606.53 (A.O.B.E.).
 3. REFER TO DWG. NO. WZP-01 FOR WORK ZONE STAGING TYPICAL SECTION A-A, MIRROR FOR WESTBOUND.

PREPARED BY: JEFFREY W. PANGBURN, P.E. ALTERED BY: ON:



AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:	NY ROUTE 74 OVER EAGLE LAKE CHANNEL	PIN 1110.02	BRIDGES	CULVERTS	ALL DIMENSIONS IN FT UNLESS OTHERWISE NOTED	CONTRACT NUMBER D264836
	CULVERT REPLACEMENT	UTILITY QUALITY LEVEL "C"	1080770	C120086		
	SH 8230 SCHROON LAKE - TICONDEROGA, PART 2				WORK ZONE TRAFFIC CONTROL PLAN WESTBOUND CLOSURE	DRAWING NO. WZP-03 SHEET NO. 10
	TOWN OF TICONDEROGA					
	COUNTY: ESSEX COUNTY REGION: 1					

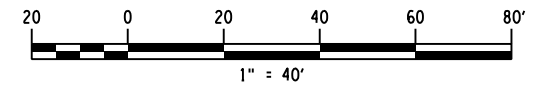
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TRAFFIC SIGNAL LEGEND

←	SIGNAL HEAD
■	MICROWAVE DETECTOR
Ⓛ	DETECTOR NUMBER
Ⓢ	PHASE NUMBER

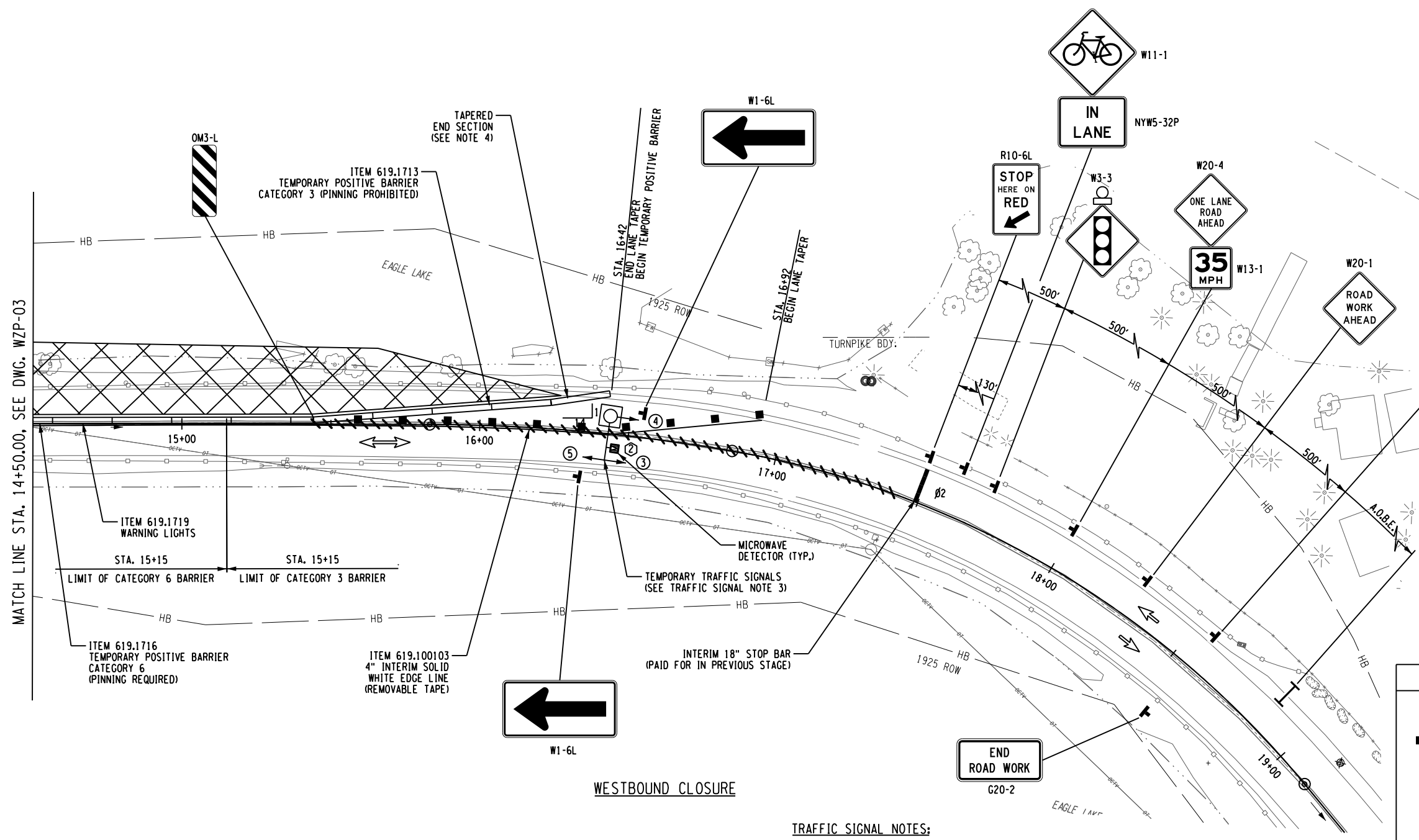
LEGEND

+	WORK ZONE SIGN LOCATION
■	CHANNELIZING DEVICE
///	PAVEMENT MARKING COVER ITEM 619.0803
→	DIRECTION OF TRAVEL
I	PORTABLE VARIABLE MESSAGE SIGN ITEM 619.11113
⊠	WORK ZONE



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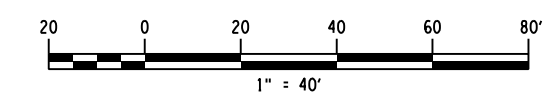
DESIGN SUPERVISOR J. PANGBURN
 JOB MANAGER C. GEROUX
 DESIGN L. SHORT
 CHECK A. BROWN
 DRAFTING K. DETRICK
 CHECK A. BROWN
 PROJECT MANAGER C. GEROUX



STATE LAW
 LICENSE
 SUSPENDED
 AFTER TWO
 WORK ZONE
 SPEEDING
 TICKETS

TRAFFIC SIGNAL LEGEND	
←	SIGNAL HEAD
■	MICROWAVE DETECTOR
①	DETECTOR NUMBER
Ø	PHASE NUMBER

LEGEND	
+	WORK ZONE SIGN LOCATION
■	CHANNELIZING DEVICE
///	PAVEMENT MARKING COVER ITEM 619.0803
→	DIRECTION OF TRAVEL
I	PORTABLE VARIABLE MESSAGE SIGN ITEM 619.11113
⊠	WORK ZONE



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PREPARED BY: JEFFREY W. PANGBURN, P.E. ALTERED BY: ON:



AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:	NY ROUTE 74 OVER EAGLE LAKE CHANNEL CULVERT REPLACEMENT SH 8230 SCHROON LAKE - TICONDEROGA, PART 2 TOWN OF TICONDEROGA COUNTY: ESSEX COUNTY REGION: 1	PIN 1110.02 UTILITY QUALITY LEVEL "C"	BRIDGES 1080770	CULVERTS C120086	ALL DIMENSIONS IN FT UNLESS OTHERWISE NOTED	CONTRACT NUMBER D264836
WORK ZONE TRAFFIC CONTROL PLAN WESTBOUND CLOSURE					DRAWING NO. WZP-04 SHEET NO. 11	

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY, IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.



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PROJECT MANAGER C. GEROUX
 CHECK A. BROWN
 DRAFTING K. DETRICK
 CHECK A. BROWN
 DESIGN L. SHORT
 JOB MANAGER C. GEROUX
 DESIGN SUPERVISOR J. PANGBURN

DETOUR M4-8 EAST M3-2 74 M1-4 → M6-1R ①	DETOUR M4-8 EAST M3-2 74 M1-4 ← M6-1L ②	DETOUR M4-8 EAST M3-2 74 M1-4 ↑ M6-3 ③	DETOUR M4-8 EAST M3-2 74 M1-4 ↗ M6-2R ④	DETOUR M4-8 EAST M3-2 74 M1-4 ↖ M6-2L ⑤	DETOUR M4-8 WEST M3-4 74 M1-4 → M6-1R ⑥	DETOUR M4-8 WEST M3-4 74 M1-4 ← M6-1L ⑦	DETOUR M4-8 WEST M3-4 74 M1-4 ↑ M6-3 ⑧	DETOUR M4-8 WEST M3-4 74 M1-4 ↗ M6-2R ⑨	DETOUR M4-8 WEST M3-4 74 M1-4 ↖ M6-2L ⑩
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DETOUR M4-8 EAST M3-2 74 M1-4 ← M5-1L ⑪	DETOUR M4-8 EAST M3-2 74 M1-4 → M5-1R ⑫	END DETOUR M4-8A EAST M3-2 74 M1-4 ⑬	DETOUR M4-8 WEST M3-4 74 M1-4 ← M5-1L ⑭	DETOUR M4-8 WEST M3-4 74 M1-4 → M5-1R ⑮	END DETOUR M4-8A WEST M3-4 74 M1-4 ⑯	DETOUR M4-8 WEST M3-4 74 M1-4 ↗ M5-2R ⑰	DETOUR M4-8 WEST M3-4 74 M1-4 ↖ M5-2L ⑱	DETOUR M4-8 EAST M3-2 74 M1-4 ↗ M5-2R ⑲	DETOUR M4-8 EAST M3-2 74 M1-4 ↖ M5-2L ⑳
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 W20-2 A	 ROAD CLOSED 10 MILES AHEAD LOCAL TRAFFIC ONLY R11-3 M4-10L B	 ROAD CLOSED 10 MILES AHEAD LOCAL TRAFFIC ONLY R11-3 C
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LEGEND	
	DETOUR SIGN LOCATION
	DETOUR ROUTE
	PORTABLE VARIABLE MESSAGE SIGN (A.O.B.E.)
	WORK ZONE

PREPARED BY: JEFFREY W. PANGBURN, P.E. ALTERED BY: ON:

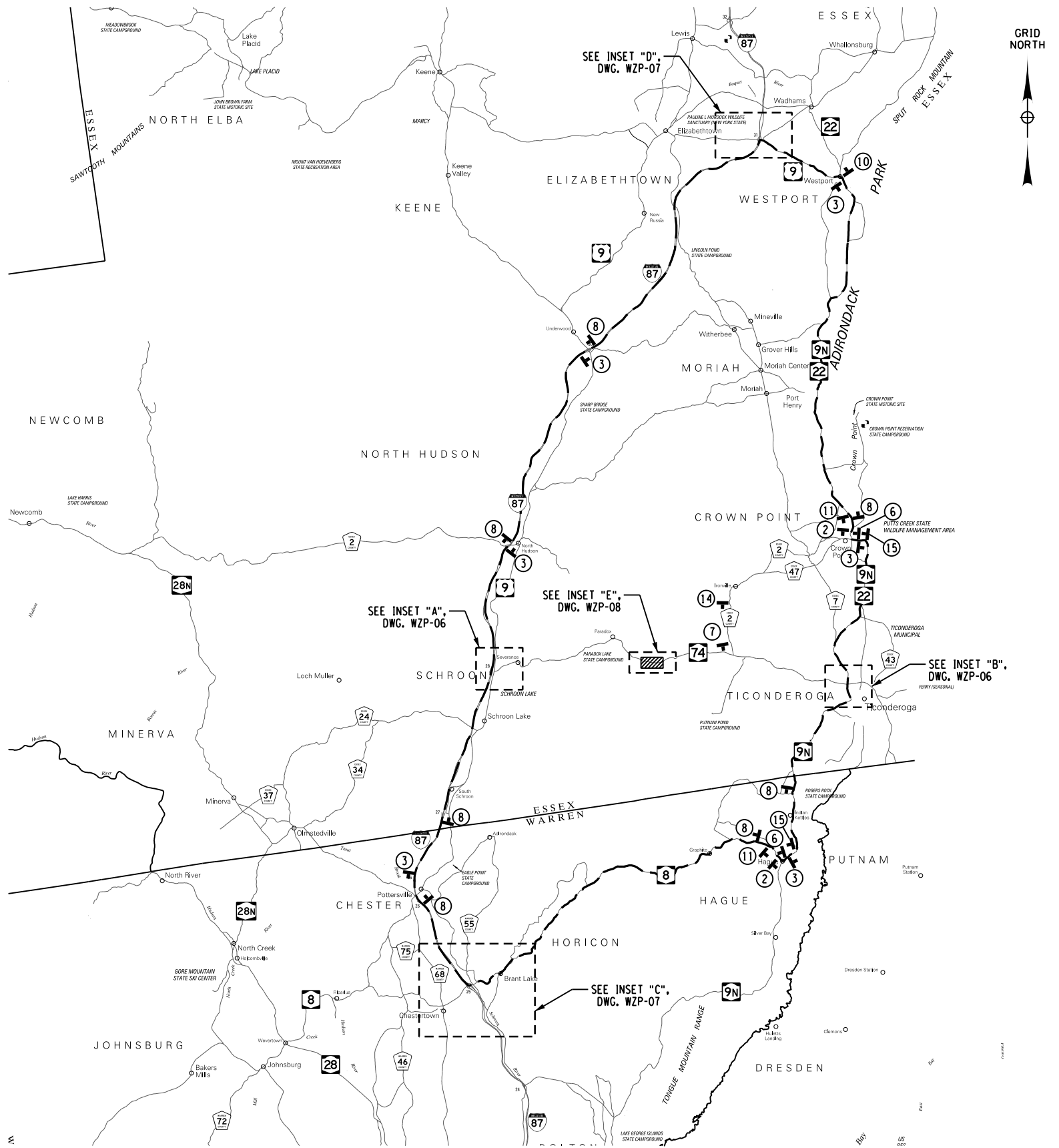


AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:	NY ROUTE 74 OVER EAGLE LAKE CHANNEL CULVERT REPLACEMENT SH 8230 SCHROON LAKE - TICONDEROGA, PART 2 TOWN OF TICONDEROGA COUNTY: ESSEX COUNTY REGION: 1
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PIN 1110.02 UTILITY QUALITY LEVEL "C"	BRIDGES 1080770	CULVERTS C120086
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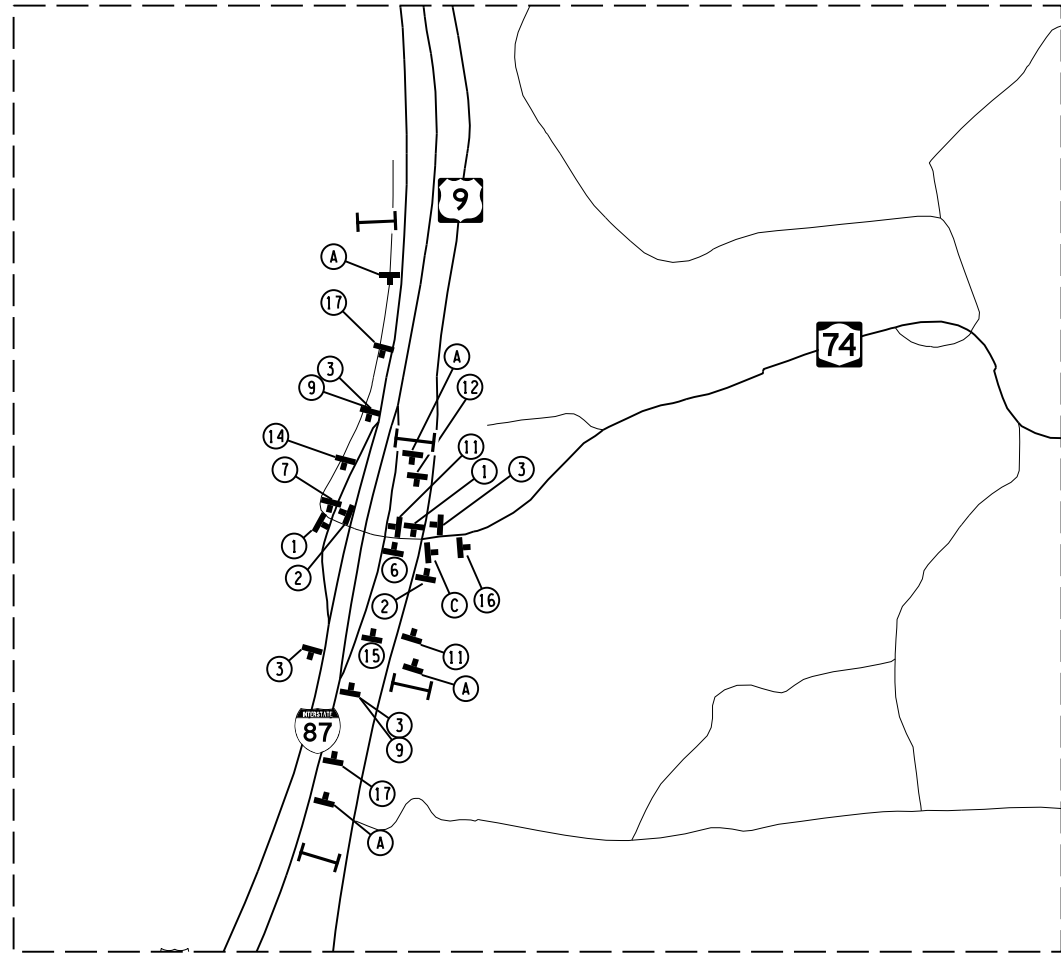
ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED		CONTRACT NUMBER D264836
WORK ZONE TRAFFIC CONTROL PLAN DETOUR PLAN		DRAWING NO. WZP-05 SHEET NO. 12

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

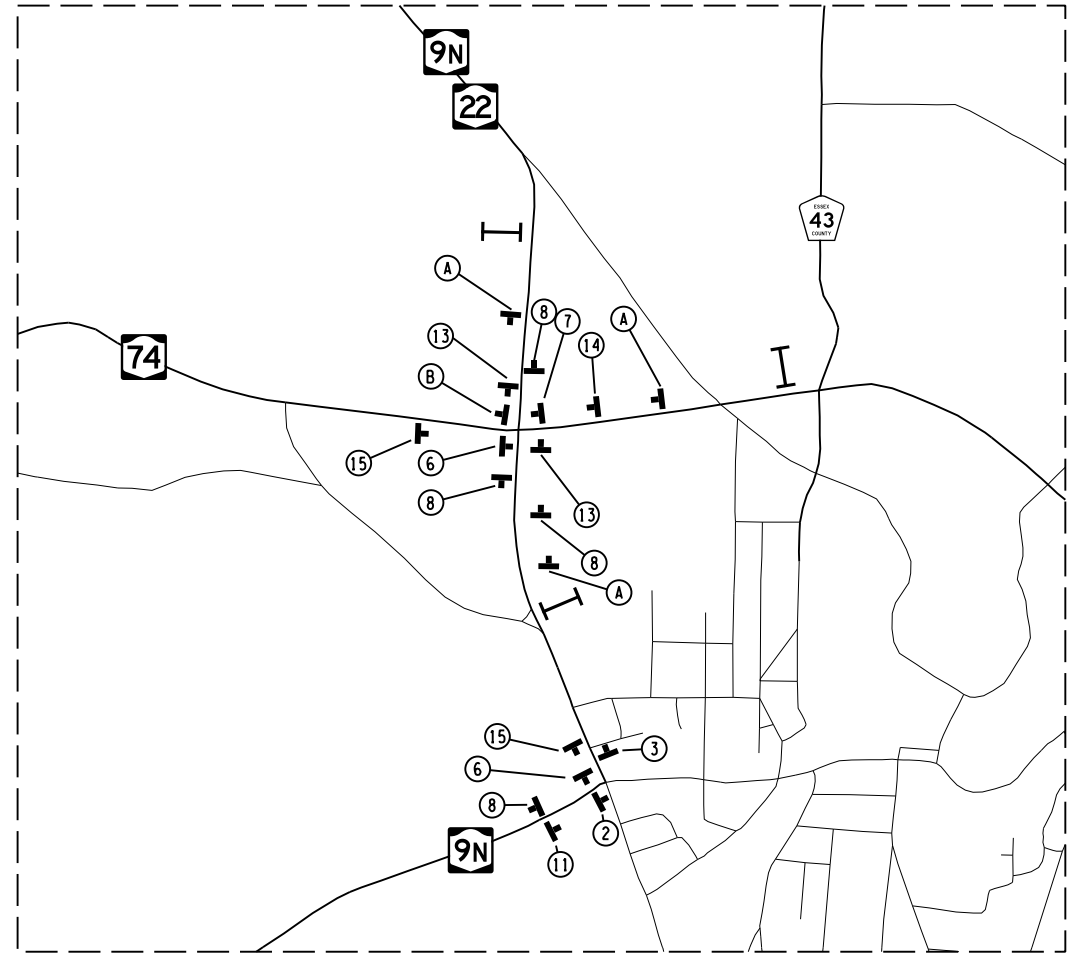


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DESIGN SUPERVISOR J. PANGBURN JOB MANAGER C. GEROUX DESIGN L. SHORT DRAFTING K. DETRICK CHECK A. BROWN CHECK A. BROWN PROJECT MANAGER C. GEROUX



INSET "A"
(NOT TO SCALE)



INSET "B"
(NOT TO SCALE)



LEGEND	
	DETOUR SIGN LOCATION
	PORTABLE VARIABLE MESSAGE SIGN (A.O.B.E.)
	DETOUR ROUTE

TABLE OF PORTABLE VARIABLE MESSAGE SIGNS (SEE PLANS FOR LOCATIONS)		
DATES REQUIRED	MESSAGE 1	MESSAGE 2
1 WEEK PRIOR TO CLOSURE	RT 74 AT EAGLE LK TO CLOSE	FROM X/XX-X/XX
DURING CLOSURE	RT 74 AT EAGLE LK CLOSED	FOLLOW DETOUR

PREPARED BY: JEFFREY W. PANGBURN, P.E. ALTERED BY: ON:



NOTE:
 1. EXACT LOCATION OF PORTABLE VARIABLE MESSAGE SIGN TO BE DETERMINED IN THE FIELD A.O.B.E.

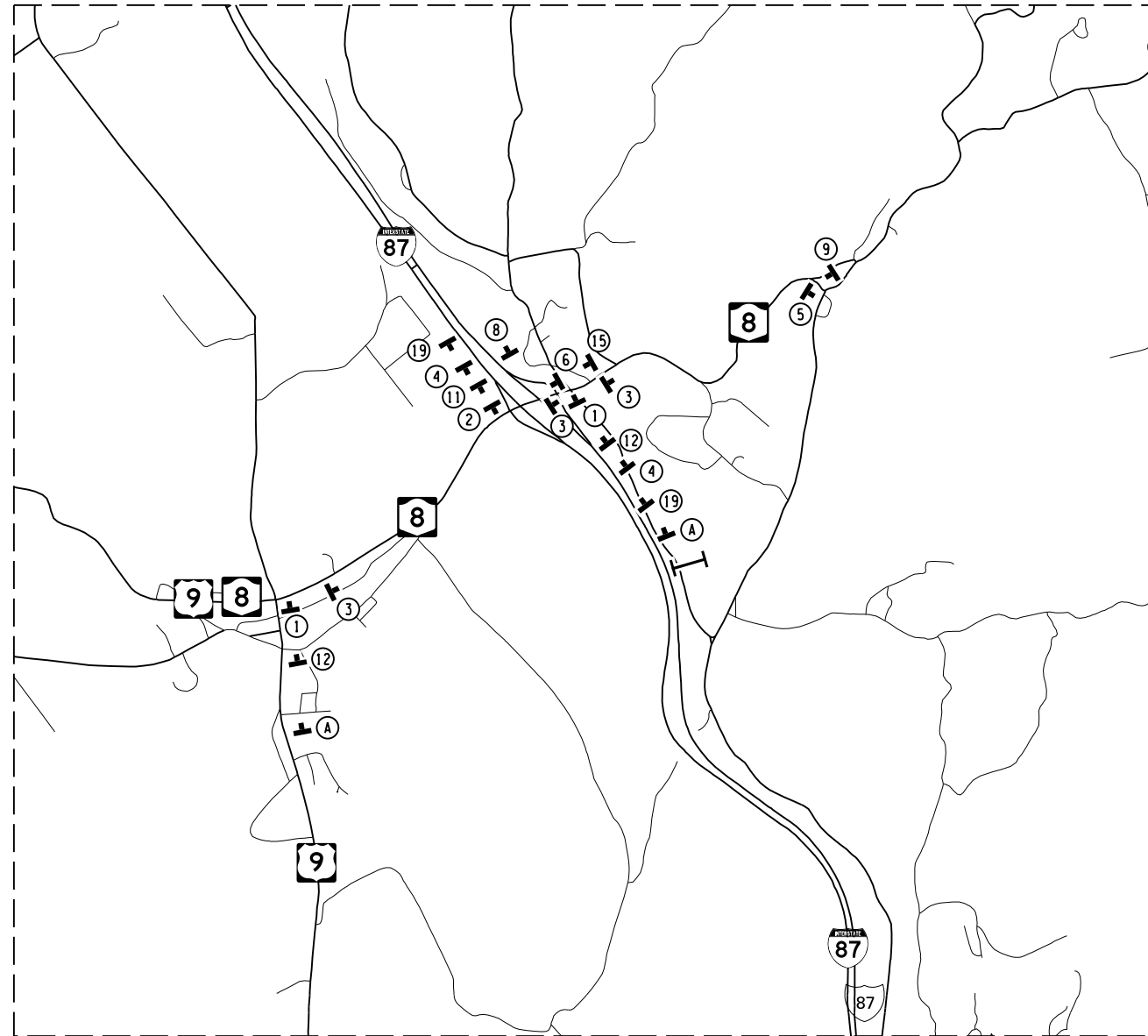
AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:	NY ROUTE 74 OVER EAGLE LAKE CHANNEL	PIN 1110.02	BRIDGES	CULVERTS	ALL DIMENSIONS IN FT UNLESS OTHERWISE NOTED	CONTRACT NUMBER
	CULVERT REPLACEMENT	UTILITY QUALITY LEVEL "C"	1080770	C120086		D264836
	SH 8230 SCHROON LAKE - TICONDEROGA, PART 2				WORK ZONE TRAFFIC CONTROL PLAN DETOUR PLAN	DRAWING NO. WZP-06
	TOWN OF TICONDEROGA					SHEET NO. 13
	COUNTY: ESSEX COUNTY REGION: 1					

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY, IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

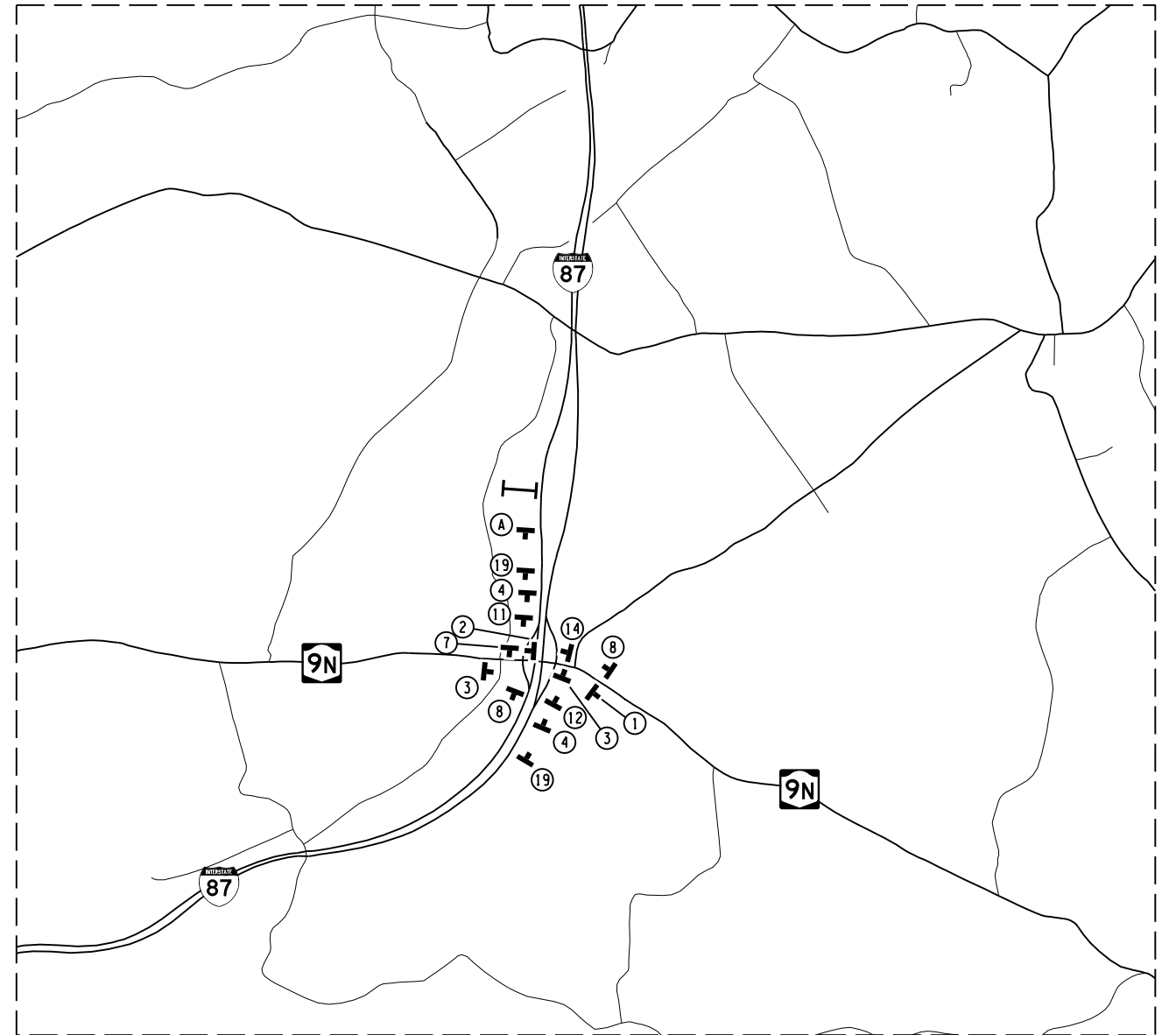


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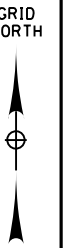
DESIGN SUPERVISOR J. PANGBURN JOB MANAGER C. GEROUX DESIGN L. SHORT DRAFTING K. DETRICK CHECK A. BROWN CHECK A. BROWN PROJECT MANAGER C. GEROUX



INSET "C"
(NOT TO SCALE)



INSET "D"
(NOT TO SCALE)



LEGEND	
	DETOUR SIGN LOCATION
	TYPE III BARRICADE
	PORTABLE VARIABLE MESSAGE SIGN (A.O.B.E.)
	DETOUR ROUTE

PREPARED BY: JEFFREY W. PANGBURN, P.E. ALTERED BY:
ON:



NOTE:
1. EXACT LOCATION OF PORTABLE VARIABLE MESSAGE SIGN TO BE DETERMINED IN THE FIELD A.O.B.E.

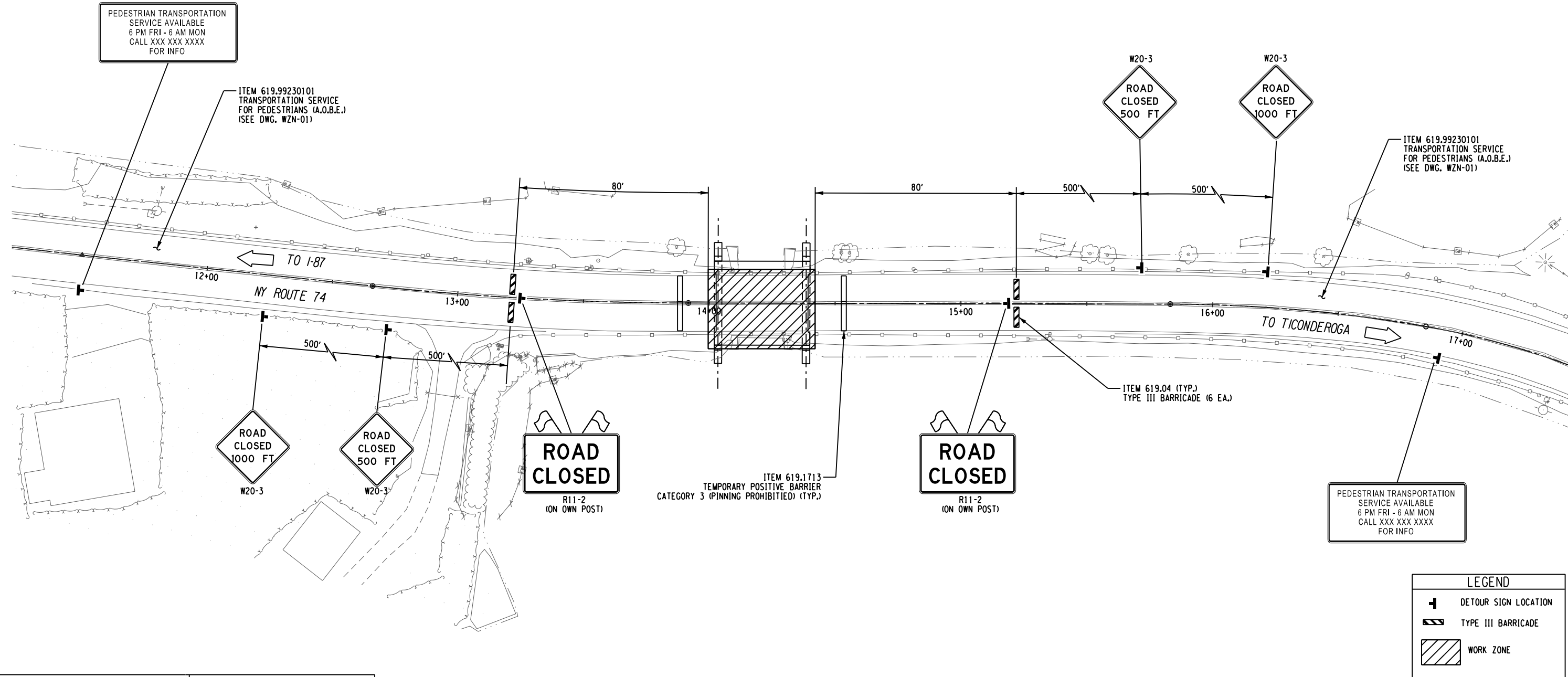
AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:	NY ROUTE 74 OVER EAGLE LAKE CHANNEL	PIN 1110.02	BRIDGES	CULVERTS	ALL DIMENSIONS IN FT UNLESS OTHERWISE NOTED	CONTRACT NUMBER D264836
	CULVERT REPLACEMENT	UTILITY QUALITY LEVEL "C"	1080770	C120086		
	SH 8230 SCHROON LAKE - TICONDEROGA, PART 2				WORK ZONE TRAFFIC CONTROL PLAN DETOUR PLAN	DRAWING NO. WZP-07 SHEET NO. 14
	TOWN OF TICONDEROGA					
	COUNTY: ESSEX COUNTY REGION: 1					

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.



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DESIGN SUPERVISOR J. PANGBURN
 JOB MANAGER C. GEROUX
 DESIGN L. SHORT
 CHECK A. BROWN
 DRAFTING K. DETRICK
 CHECK A. BROWN
 PROJECT MANAGER C. GEROUX



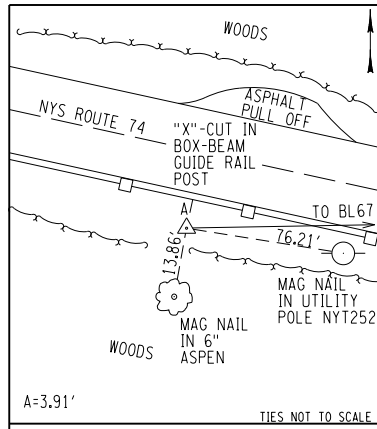
PREPARED BY: JEFFREY W. PANGBURN, P.E. ALTERED BY: ON:



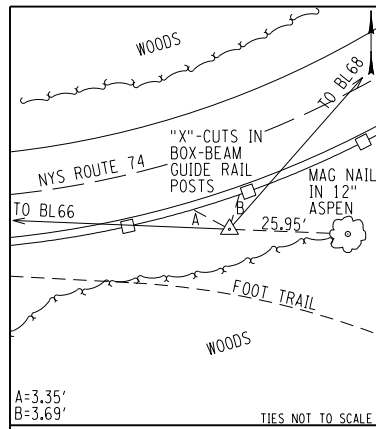
AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:	NY ROUTE 74 OVER EAGLE LAKE CHANNEL	PIN 1110.02	BRIDGES 1080770	CULVERTS C120086	ALL DIMENSIONS IN FT UNLESS OTHERWISE NOTED	CONTRACT NUMBER D264836
	CULVERT REPLACEMENT	UTILITY QUALITY LEVEL "C"				WORK ZONE TRAFFIC CONTROL PLAN CLOSURE PLAN
	SH 8230 SCHROON LAKE - TICONDEROGA, PART 2	TOWN OF TICONDEROGA				
	COUNTY: ESSEX COUNTY	REGION: 1				

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

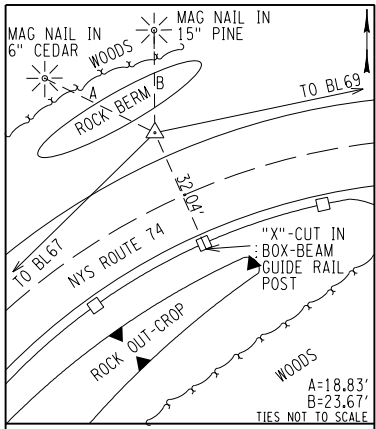




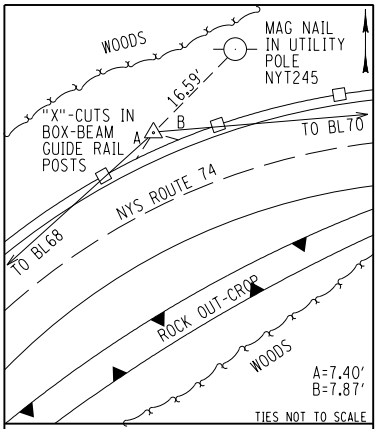
66 STA. 10+00.00
 YELLOW CAPPED IRON ROD SET
 20'± S'LY OF CENTERLINE OF
 ROUTE 74, AND 36'± E'LY OF
 REFERENCE MARKER 74-1201-1118
 NAD83 (2011) ZONE: EAST
 N: 1837960.8508
 E: 728586.9828
 ELEV: 992.50



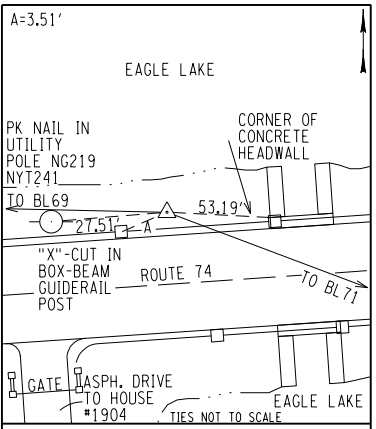
67 STA. 14+31.52
 YELLOW CAPPED IRON ROD SET
 17'± S'LY OF CENTERLINE OF
 ROUTE 74, AND 166'± W'LY OF
 REFERENCE MARKER 74-1201-1117
 NAD83 (2011) ZONE: EAST
 N: 1838071.3238
 E: 729004.1271
 ELEV: 986.30



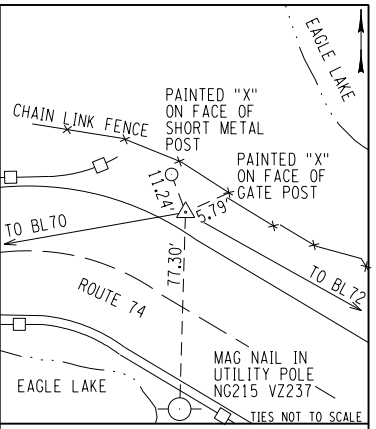
68 STA. 20+12.69
 YELLOW CAPPED IRON ROD SET
 17'± NW'LY OF CENTERLINE OF
 ROUTE 74, AND 99'± NE'LY OF
 REFERENCE MARKER 74-1201-1116
 NAD83 (2011) ZONE: EAST
 N: 1838466.5963
 E: 729430.1731
 ELEV: 992.39



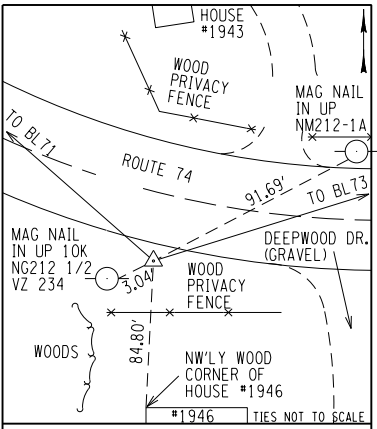
69 STA. 27+74.05
 YELLOW CAPPED IRON ROD SET
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 ROUTE 74, AND 218'± W'LY OF
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 NAD83 (2011) ZONE: EAST
 N: 1838852.9625
 E: 730086.2084
 ELEV: 980.30



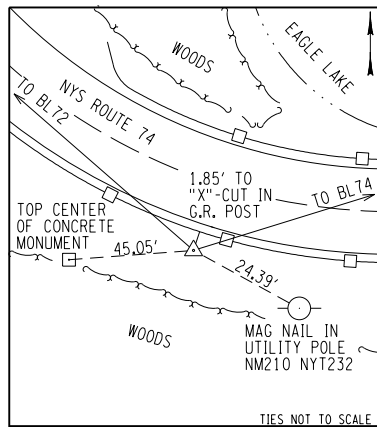
70 STA. 36+02.37
 YELLOW CAPPED IRON ROD SET
 15' N'LY OF CENTERLINE OF
 ROUTE 74, AND 37' E'LY OF
 REFERENCE MARKER 74-1201-1113
 NAD83 (2011) ZONE: EAST
 N: 1839024.7657
 E: 730896.5171
 ELEV: 951.21



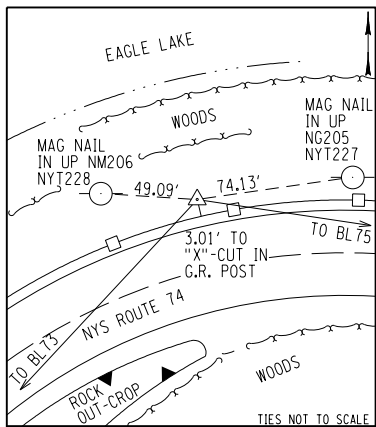
71 STA. 41+86.01
 YELLOW CAPPED IRON ROD SET
 22' NE'LY OF CENTERLINE OF
 ROUTE 74, AND 89' SE'LY OF
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 NAD83 (2011) ZONE: EAST
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 E: 731480.1584
 ELEV: 951.45



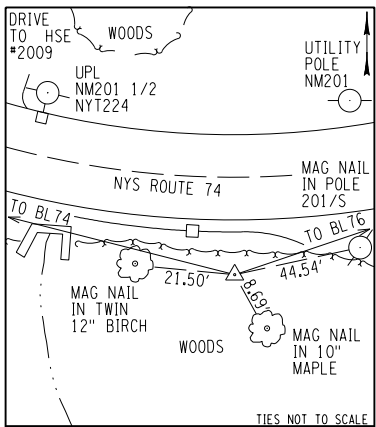
72 STA. 45+61.68
 YELLOW CAPPED IRON ROD SET
 20' S'LY OF CENTERLINE OF
 ROUTE 74, AND 114' W'LY OF
 REFERENCE MARKER 74-1201-1111
 NAD83 (2011) ZONE: EAST
 N: 1838805.4768
 E: 731786.7962
 ELEV: 962.98



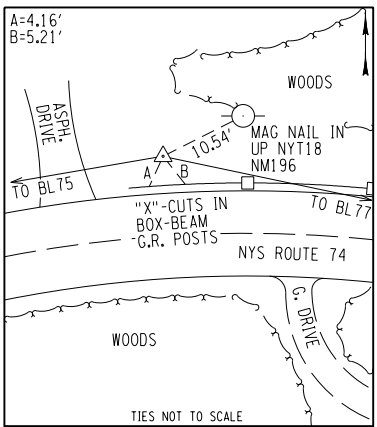
73 STA. 48+42.44
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 ROUTE 74, AND 170'± E'LY OF
 REFERENCE MARKER 74-1201-1111
 NAD83 (2011) ZONE: EAST
 N: 1838848.6077
 E: 732064.2143
 ELEV: 958.41



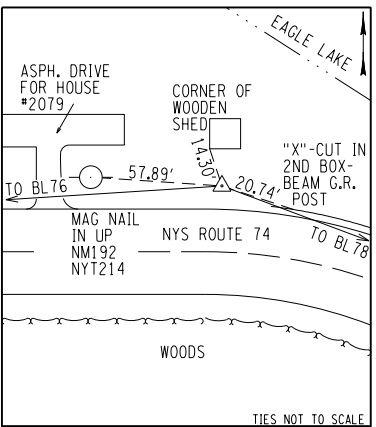
74 STA. 54+69.65
 YELLOW CAPPED IRON ROD SET
 21'± N'LY OF CENTERLINE OF
 ROUTE 74, AND 169'± W'LY OF
 REFERENCE MARKER 74-1201-1109
 NAD83 (2011) ZONE: EAST
 N: 1839340.7341
 E: 732453.0782
 ELEV: 955.30



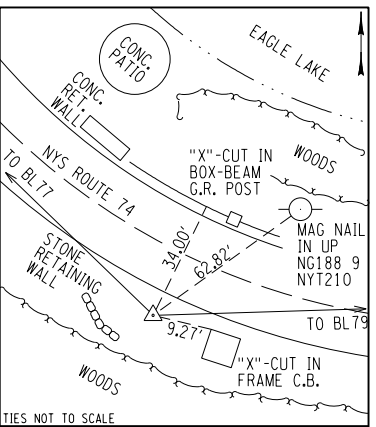
75 STA. 63+05.54
 YELLOW CAPPED IRON ROD SET
 30'± S'LY OF CENTERLINE OF
 ROUTE 74, AND 24'± E'LY OF
 REFERENCE MARKER 74-1201-1108
 NAD83 (2011) ZONE: EAST
 N: 1839474.5629
 E: 733278.1854
 ELEV: 960.19



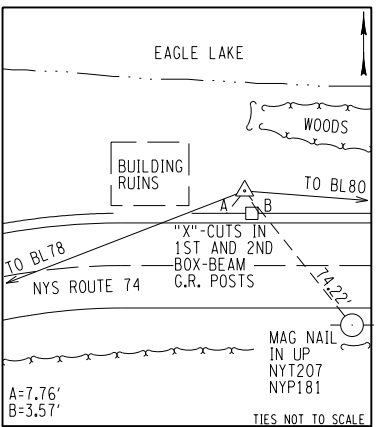
76 STA. 70+08.54
 YELLOW CAPPED IRON ROD SET
 19'± N'LY OF CENTERLINE OF
 ROUTE 74, AND 76'± W'LY OF
 REFERENCE MARKER 74-1201-1106
 NAD83 (2011) ZONE: EAST
 N: 1839791.3590
 E: 733905.7577
 ELEV: 959.26



77 STA. 80+28.26
 YELLOW CAPPED IRON ROD SET
 18'± N'LY OF CENTERLINE OF
 ROUTE 74, AND 144'± W'LY OF
 REFERENCE MARKER 74-1201-1104
 NAD83 (2011) ZONE: EAST
 N: 1840022.6927
 E: 734898.8861
 ELEV: 956.43



78 STA. 87+57.14
 YELLOW CAPPED IRON ROD SET
 18'± S'LY OF CENTERLINE OF
 ROUTE 74, AND 75'± E'LY OF
 REFERENCE MARKER 74-1201-1103
 NAD83 (2011) ZONE: EAST
 N: 1840026.2757
 E: 735627.7594
 ELEV: 956.95



79 STA. 94+38.85
 YELLOW CAPPED IRON ROD SET
 16'± N'LY OF CENTERLINE OF
 ROUTE 74, AND 239'± E'LY OF
 REFERENCE MARKER 74-1201-1102
 NAD83 (2011) ZONE: EAST
 N: 1840233.0370
 E: 736277.3615
 ELEV: 958.48

PREPARED BY: Brian F. Magee
 ALTERED BY: ON:

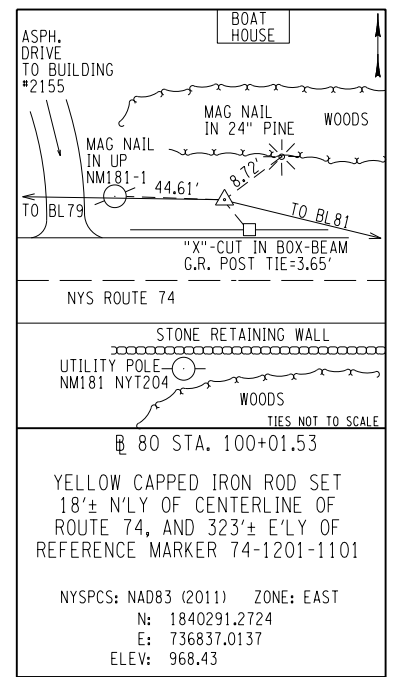


AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:	NY ROUTE 74 OVER EAGLE LAKE CHANNEL	PIN 1110.02	BRIDGES	1080770	CULVERTS	C120086	ALL DIMENSIONS IN FT UNLESS OTHERWISE NOTED	CONTRACT NUMBER	D264836
	CULVERT REPLACEMENT	UTILITY QUALITY LEVEL "C"					MISCELLANEOUS TABLES BASELINE TIES	DRAWING NO.	MST-01
	SH 8230 SCHROON LAKE - TICONDEROGA, PART 2							SHEET NO.	16
	TOWN OF TICONDEROGA								
	COUNTY: ESSEX COUNTY	REGION: 1							

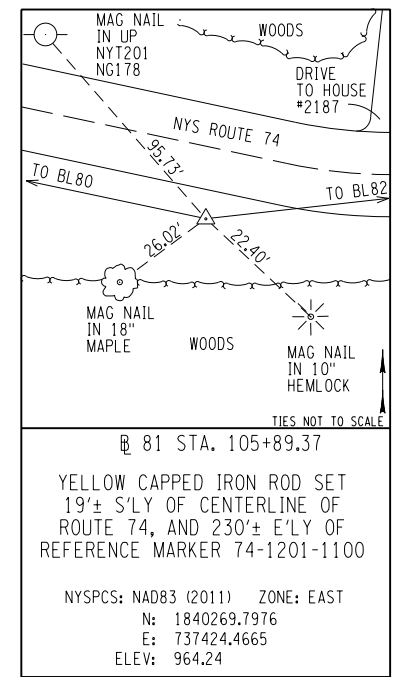
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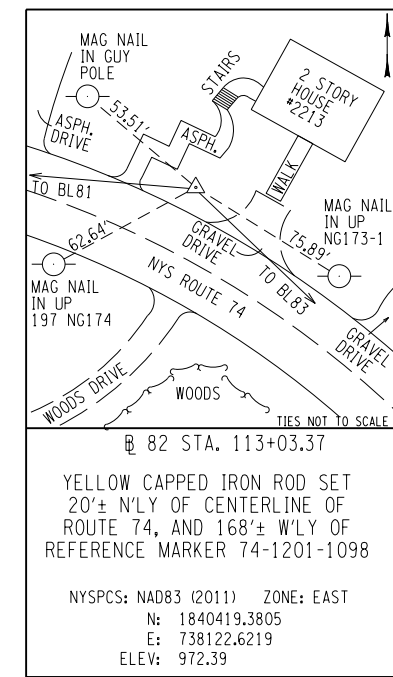
PROJECT MANAGER C. GEROUX
 CHECK A. BROWN
 DRAFTING K. DETRICK
 CHECK A. BROWN
 DESIGN L. SHORT
 JOB MANAGER C. GEROUX
 DESIGN SUPERVISOR J. PANGBURN



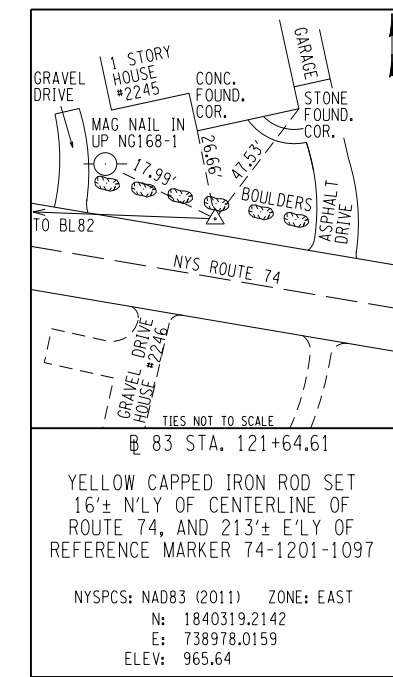
BM 80 STA. 100+01.53
 YELLOW CAPPED IRON ROD SET 18'± N'LY OF CENTERLINE OF ROUTE 74, AND 323'± E'LY OF REFERENCE MARKER 74-1201-1101
 NYSPCS: NAD83 (2011) ZONE: EAST
 N: 1840291.2724
 E: 736837.0137
 ELEV: 968.43



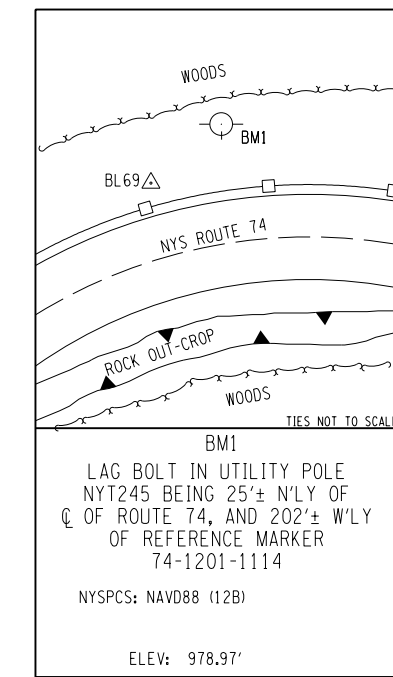
BM 81 STA. 105+89.37
 YELLOW CAPPED IRON ROD SET 19'± S'LY OF CENTERLINE OF ROUTE 74, AND 230'± E'LY OF REFERENCE MARKER 74-1201-1100
 NYSPCS: NAD83 (2011) ZONE: EAST
 N: 1840269.7976
 E: 737424.4665
 ELEV: 964.24



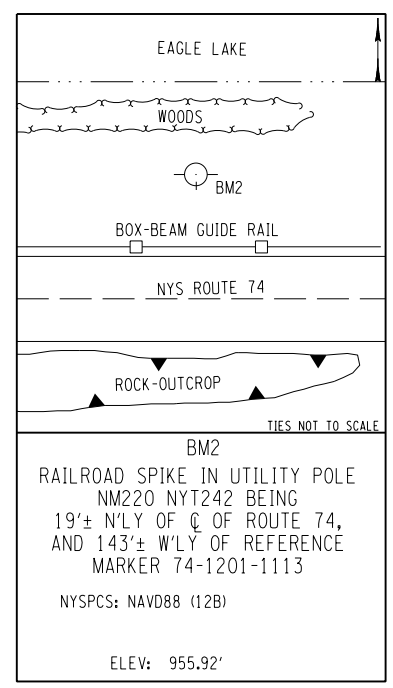
BM 82 STA. 113+03.37
 YELLOW CAPPED IRON ROD SET 20'± N'LY OF CENTERLINE OF ROUTE 74, AND 168'± W'LY OF REFERENCE MARKER 74-1201-1098
 NYSPCS: NAD83 (2011) ZONE: EAST
 N: 1840419.3805
 E: 738122.6219
 ELEV: 972.39



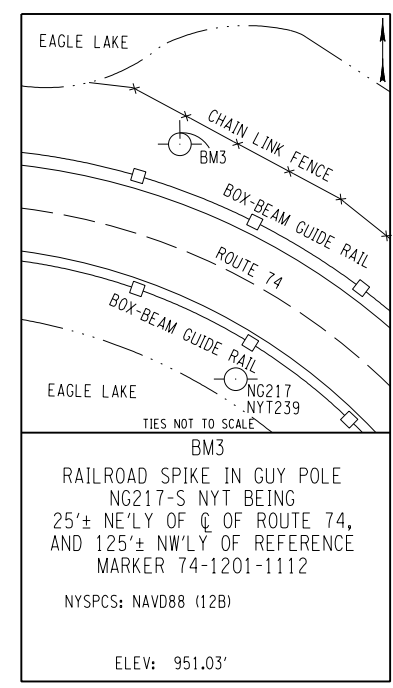
BM 83 STA. 121+64.61
 YELLOW CAPPED IRON ROD SET 16'± N'LY OF CENTERLINE OF ROUTE 74, AND 213'± E'LY OF REFERENCE MARKER 74-1201-1097
 NYSPCS: NAD83 (2011) ZONE: EAST
 N: 1840319.2142
 E: 738978.0159
 ELEV: 965.64



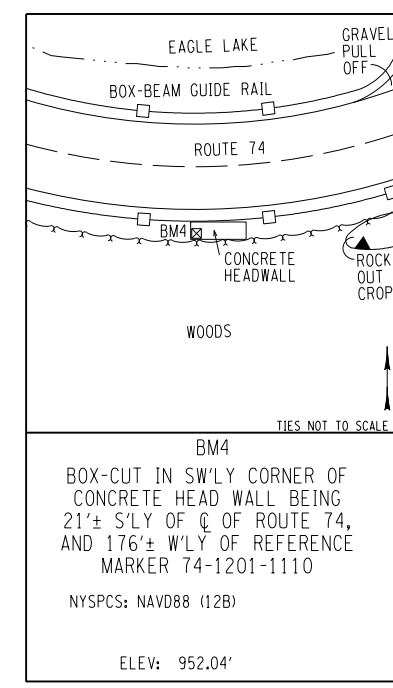
BM1
 LAG BOLT IN UTILITY POLE NYT245 BEING 25'± N'LY OF Q OF ROUTE 74, AND 202'± W'LY OF REFERENCE MARKER 74-1201-1114
 NYSPCS: NAVD88 (12B)
 ELEV: 978.97'



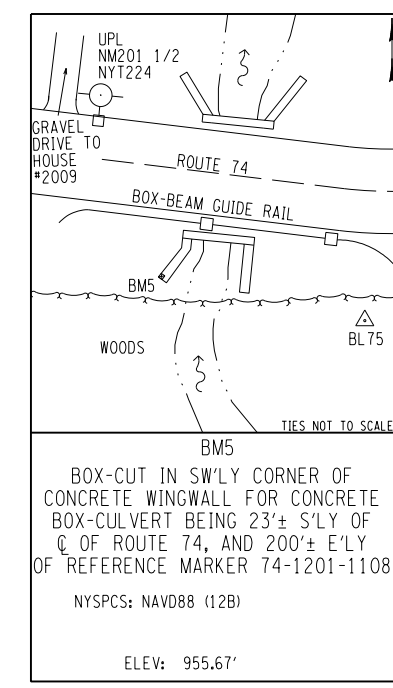
BM2
 RAILROAD SPIKE IN UTILITY POLE NM220 NYT242 BEING 19'± N'LY OF Q OF ROUTE 74, AND 143'± W'LY OF REFERENCE MARKER 74-1201-1113
 NYSPCS: NAVD88 (12B)
 ELEV: 955.92'



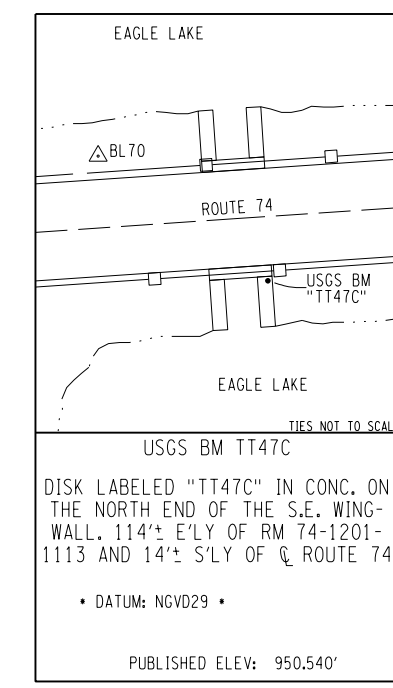
BM3
 RAILROAD SPIKE IN GUY POLE NG217-S NYT BEING 25'± NE'LY OF Q OF ROUTE 74, AND 125'± NW'LY OF REFERENCE MARKER 74-1201-1112
 NYSPCS: NAVD88 (12B)
 ELEV: 951.03'



BM4
 BOX-CUT IN SW'LY CORNER OF CONCRETE HEAD WALL BEING 21'± S'LY OF Q OF ROUTE 74, AND 176'± W'LY OF REFERENCE MARKER 74-1201-1110
 NYSPCS: NAVD88 (12B)
 ELEV: 952.04'



BM5
 BOX-CUT IN SW'LY CORNER OF CONCRETE WINGWALL FOR CONCRETE BOX-CULVERT BEING 23'± S'LY OF Q OF ROUTE 74, AND 200'± E'LY OF REFERENCE MARKER 74-1201-1108
 NYSPCS: NAVD88 (12B)
 ELEV: 955.67'



USGS BM TT47C
 DISK LABELED "TT47C" IN CONC. ON THE NORTH END OF THE S.E. WING-WALL. 114'± E'LY OF RM 74-1201-1113 AND 14'± S'LY OF Q OF ROUTE 74
 • DATUM: NGVD29 •
 PUBLISHED ELEV: 950.540'

PROJECT SURVEY CONTROL P.I.N. 1110.02
 BASE MAPPING OF THE PROJECT SITE IS FROM AERIAL PHOTOGRAPHY TAKEN NOVEMBER 9, 2018 WITH SURVEY CONTROL COMPLETED DECEMBER 2018.
 ESSEX COUNTY.
 UNITS: USG. (FEET).
 THE PROJECT SURVEY HORIZONTAL AND VERTICAL CONTROL WAS ESTABLISHED BY USE OF GPS STATIC OBSERVATION SURVEY METHODS.
 THE HORIZONTAL DATUM IS THE NORTH AMERICAN DATUM OF 1983, 2011 ADJUSTMENT: NAD 83(2011).
 THE COORDINATE SYSTEM IS THE NEW YORK STATE PLANE COORDINATE SYSTEM, EASTERN ZONE 3101.
 SURVEY BASELINE DISTANCES ARE REDUCED TO THE STATE PLANE COORDINATE SYSTEM GRID BY AN AVERAGE COMBINED SCALE FACTOR (GRID FACTOR x ELEVATION FACTOR) OF:
 GRID FACTOR X ELEVATION FACTOR = COMBINED SCALE FACTOR
 0.9999673256 X 0.9999586252 = 0.9999259521
 THE NGS AND NYSNET CORS STATIONS USED TO ESTABLISH HORIZONTAL CONTROL ARE:

SITE NUMBER	DESIGNATION	NORTHING	EASTING
0046	NVIL	1803287.0499	550818.5422
N/A	HDF5	1618119.4834	748025.8535
0028	NYET	1960321.5970	743691.3340
0038	VTRU	1742907.3248	894848.7898

 THE VERTICAL DATUM IS THE NORTH AMERICAN VERTICAL DATUM OF 1988 (NAVD 88) USING GEOID 12B. THE NGS AND NYSNET STATIONS AND PUBLISHED ELLIPSOID HEIGHTS USED TO ESTABLISH VERTICAL CONTROL VALUES ARE:

SITE NUMBER	DESIGNATION	ELEVATION
0046	NVIL	1739.60'
N/A	HDF5	240.24'
0028	NYET	666.17'
0038	VTRU	622.02'

SPECIAL NOTE

SURVEY MONUMENTS
 Existing U.S. Geodetic Survey, U.S. Coast and Geodetic Survey, National Geodetic Survey or N.Y. Dept. of Environmental Conservation monuments which may be disturbed or destroyed during construction shall be properly referenced to temporary benchmarks before being disturbed and accurately reset in their proper position upon completion of the work.
 Under the direction of a Licensed Land Surveyor, the contractor shall establish three (3) temporary bench marks outside of the work area for each monument to be disturbed or destroyed, which shall be verified by duplicate level runs. The contractor shall provide the Regional Land Surveyor with a copy of these survey notes and inform him of the date the monument is to be disturbed or destroyed. The original plaque is to be returned to Regional Land Surveyor.
 The Regional Land Surveyor will inform the contractor of a suitable replacement location and will provide a new plaque.
 The contractor shall elevate the new monument from the previously established temporary bench marks, reversing the above procedure. Survey notes are to be provided to the Regional Land Surveyor along with the written description of the new mark location. The work shall be certified to the Department by the Licensed Land Surveyor. The cost of this work shall be included in the various items of the contract, except the cost of any necessary concrete base shall be paid under its appropriate item.

Although there may be other monuments not listed below that may be disturbed during construction, the following bench mark is likely to be disturbed:

Description of Bench Mark
 Designation: T T 47 C 1942 State: New York County: Essex
 Nearest Town: Ticonderoga
 Distance & Direction from Nearest Town: 3.5 miles east of Paradox
 Character of Mark: A standard disk
 Stamping: TT 47 C 1942
 Established by: USGS
 Published Elevation: 950.540 feet NGVD 1929
 Surveyed Elevation: 949.880 feet NAVD88 (12B)
 Published Description: Paradox, 3.5 mi. E. of, 3.7 mi W. of Chilson, in SE. corner of highway bridge over Eagle Lake; standard tablet stamped "T T 47 C 1942," painted "US 950.2 BM"

PREPARED BY: Brian F. Magee
 ALTERED BY: ON:



AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:	NY ROUTE 74 OVER EAGLE LAKE CHANNEL	PIN 1110.02 UTILITY QUALITY LEVEL "C"	BRIDGES 1080770	CULVERTS C120086	ALL DIMENSIONS IN FT UNLESS OTHERWISE NOTED	CONTRACT NUMBER D264836
	CULVERT REPLACEMENT					
	SH 8230 SCHROON LAKE - TICONDEROGA, PART 2					
	TOWN OF TICONDEROGA					
	COUNTY: ESSEX COUNTY	REGION: 1			MISCELLANEOUS TABLES BASELINE TIES	DRAWING NO. MST-02 SHEET NO. 17

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY, IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.



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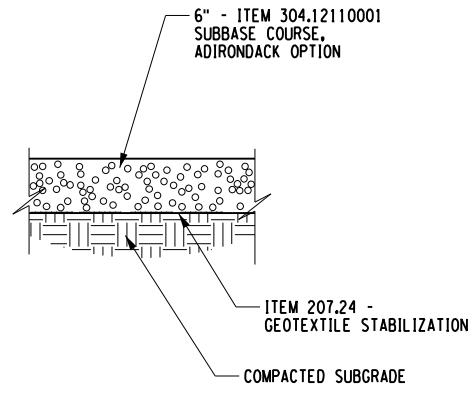
DESIGN SUPERVISOR J. PANGBURN
 DESIGN L. SHORT
 CHECK A. BROWN
 DRAFTING K. DETRICK
 CHECK A. BROWN
 PROJECT MANAGER C. GEROUX

TABLE OF HORIZONTAL ALIGNMENT				
POINT	STATION	CURVE DATA	COORDINATE	
			NORTH	EAST
NY ROUTE 74				
POB	10+00.00	AZ 81°30'29.48" LENGTH =150.00 FT	1838952.55	730549.39
PI	11+50.00	AZ 81°04'47.64" LENGTH =116.02 FT	1838974.70	730697.75
PC	12+66.03	RADIUS = 1215.00 FT DELTA =5°55'34.95" LT LENGTH = 125.67 FT TANGENT =62.89 FT	1838992.69	730812.36
PI	13+28.92		1839002.45	730874.50
PT	13+91.70	AZ 75°09'12.69" LENGTH =191.46 FT	1839018.56	730935.29
PC	15+83.16	RADIUS = 600.00 FT DELTA =9°44'54.92" RT LENGTH = 102.09 FT TANGENT =51.17 FT	1839067.62	731120.35
PI	16+34.32		1839080.73	731169.81
PCC	16+85.24	RADIUS = 320.00 FT DELTA =40°39'19.91" RT LENGTH = 227.06 FT TANGENT =118.55 FT	1839085.28	731220.78
PI	18+03.79		1839095.81	731338.86
PT	19+12.31	AZ 125°33'27.52" LENGTH =42.76 FT	1839026.87	731435.30
POE	19+55.07		1839002.01	731470.09

TABLE OF GUIDE RAIL								
STATION		SIDE	ITEM 568.51 (LF)	ITEM 568.70 (LF)	ITEM 606.10 (LF)	606.120201 (EACH)	606.53 (LF)	606.73 (LF)
FROM	TO							
12+97.00	13+69.00	LT			72.0			
13+69.00	14+01.50	LT		32.0				
14+01.50	14+40.50	LT	39.0					
14+40.50	14+72.50	LT		32.0				
14+72.50	15+17.00	LT			54.0			
13+05.92	13+34.17	RT				1		
13+34.17	13+69.79	RT			36.0			
13+69.79	14+01.50	RT		32.0				
14+01.50	14+40.50	RT	39.0					
14+40.50	14+72.50	RT		32.0				
14+72.50	15+08.50	RT			36.0			
.	.	LT					108	
.	.	RT					108	
12+97.00	14+01.50	LT						104.0
14+40.50	15+17.00	LT						86.0
13+05.92	14+01.50	RT						99.0
14+40.50	15+08.50	RT						68.0
TOTAL			78.0	128.0	198.0	1.0	216.0	357.0

TABLE OF GUIDE RAIL ITEMS AND DESCRIPTIONS		
ITEM	DESCRIPTION	UNITS
568.51	STEEL BRIDGE RAIL (FOUR-RAIL)	LF
568.70	TRANSITION BRIDGE RAILING	LF
606.10	BOX BEAM GUIDE RAIL	LF
606.120201	BOX BEAM GUIDE RAILING END ASSEMBLY, TYPE IIA	EACH
606.53	RESETTING BOX BEAM GUIDE RAILING	LF
606.73	REMOVING AND DISPOSING BOX BEAM GUIDE RAILING	LF

* ASSUMED AMOUNT OF GUIDE RAIL TO BE RESET AFTER PILE INSTALLATION. CONTRACTOR TO VERIFY ACTUAL LIMITS IN FIELD.



DRIVEWAY DETAIL
(NOT TO SCALE)

PREPARED BY: JEFFREY W. PANGBURN, P.E. ALTERED BY: ON:



AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:	NY ROUTE 74 OVER EAGLE LAKE CHANNEL	PIN 1110.02 UTILITY QUALITY LEVEL "C"	BRIDGES 1080770	CULVERTS C120086	ALL DIMENSIONS IN FT UNLESS OTHERWISE NOTED	CONTRACT NUMBER D264836
	CULVERT REPLACEMENT					MISCELLANEOUS TABLES AND DETAILS
	SH 8230 SCHROON LAKE - TICONDEROGA, PART 2					
	TOWN OF TICONDEROGA					
	COUNTY: ESSEX COUNTY REGION: 1					

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

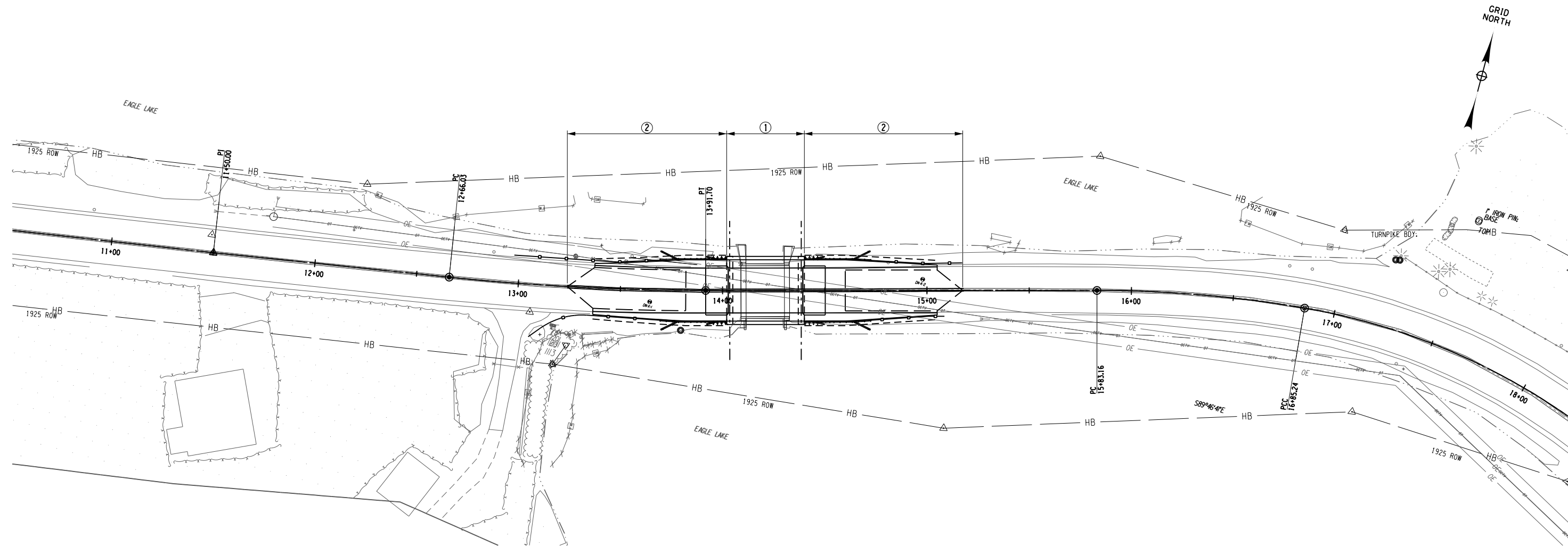


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DESIGN SUPERVISOR J. PANGBURN JOB MANAGER C. GEROUX DESIGN L. SHORT CHECK A. BROWN DRAFTING K. DETRICK CHECK A. BROWN PROJECT MANAGER C. GEROUX

TABLE OF MAINTENANCE JURISDICTION

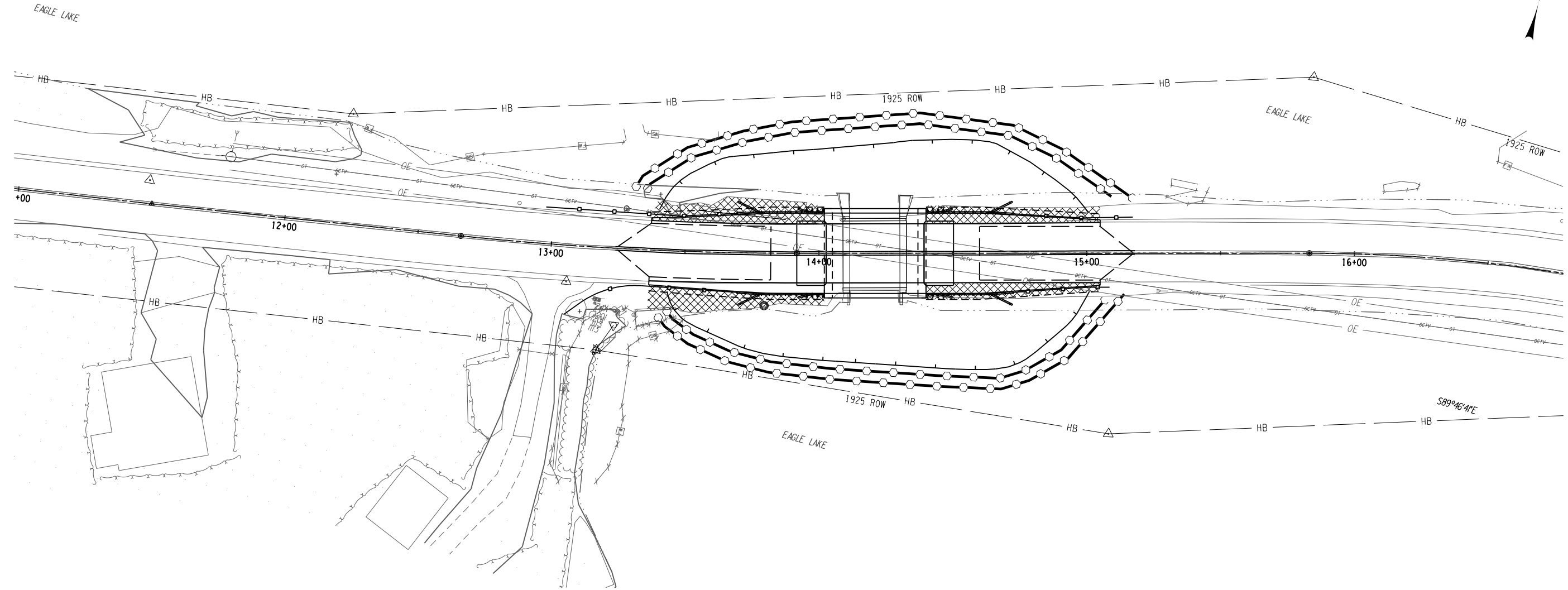
PART NO.	HIGHWAY	LIMITS	FEATURES TO BE MAINTAINED	C MILES	LANE MILES	AGENCY	AUTHORITY
HIGHWAYS							
①	NY ROUTE 74 OVER EAGLE LAKE CHANNEL	STA. 14+02 TO STA. 14+40	ENTIRE STRUCTURE	.0072	.0144	NYS DOT	HIGHWAY LAW SECTION 340 B
②	NY ROUTE 74/SH 8230	STA. 13+24 TO STA. 14+02 STA. 14+40 TO STA. 15+17	ALL FEATURES WITHIN R.O.W. EXCLUDING UTILITIES	.0294	.0587	NYS DOT	HIGHWAY LAW SECTION 12
CONTROL OF SNOW AND ICE							
①②	NY ROUTE 74/SH 8230	STA. 13+24 TO STA. 15+17	PAVEMENT AND SHOULDERS	.0366	.0731	NYS DOT	HIGHWAY LAW SECTION 12



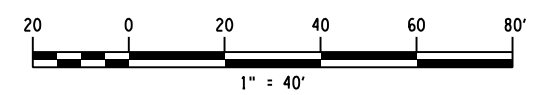
NY ROUTE 74 OVER EAGLE LAKE CHANNEL	PIN 1110.02	BRIDGES 1080770	CULVERTS C120086	ALL DIMENSIONS IN FT UNLESS OTHERWISE NOTED	CONTRACT NUMBER D264836
CULVERT REPLACEMENT	UTILITY QUALITY LEVEL "C"				DRAWING NO. MJP-01
SH 8230 SCHROON LAKE - TICONDEROGA, PART 2				MAINTENANCE JURISDICTION PLAN	SHEET NO. 19
TOWN OF TICONDEROGA					
COUNTY: ESSEX COUNTY REGION: 1					

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DESIGN SUPERVISOR J. PANGBURN JOB MANAGER C. GEROUX DESIGN L. SHORT DRAFTING K. DETRICK CHECK A. BROWN CHECK A. BROWN PROJECT MANAGER C. GEROUX



EROSION AND SEDIMENT CONTROL LEGEND	
	ITEM 209.1501 TURBIDITY CURTAIN - TEMPORARY
	LIMITS OF ITEM 201.06 CLEAR AND GRUB



PREPARED BY: JEFFREY W. PANGBURN, P.E. ALTERED BY:
ON:



AS-BUILT REVISIONS
DESCRIPTION OF ALTERATIONS:

NY ROUTE 74 OVER EAGLE LAKE CHANNEL
CULVERT REPLACEMENT
SH 8230 SCHROON LAKE - TICONDEROGA, PART 2
TOWN OF TICONDEROGA
COUNTY: ESSEX COUNTY REGION: 1

PIN 1110.02	BRIDGES	CULVERTS
UTILITY QUALITY LEVEL "C"	1080770	C120086

ALL DIMENSIONS IN FT UNLESS OTHERWISE NOTED

EROSION AND SEDIMENT CONTROL PLAN

CONTRACT NUMBER
D264836

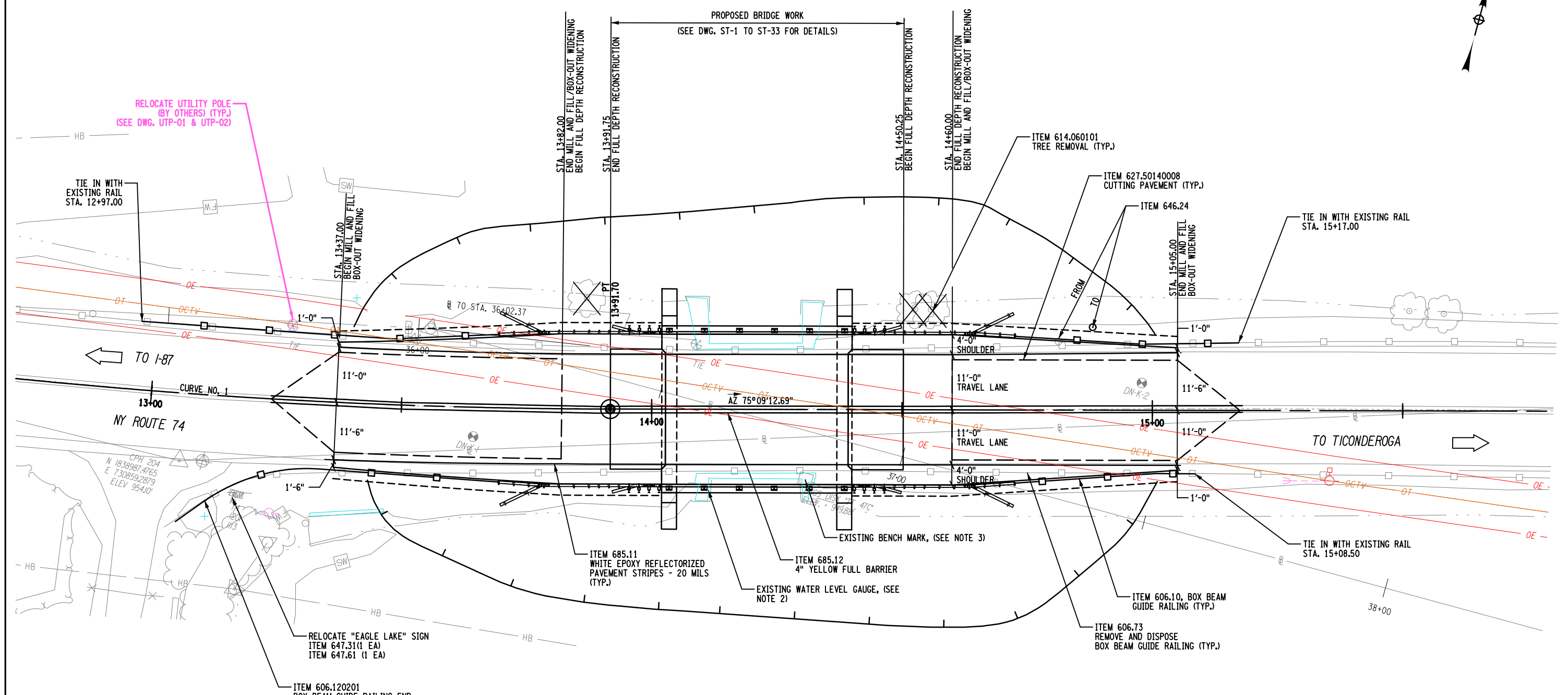
DRAWING NO. ECP-01
SHEET NO. 20

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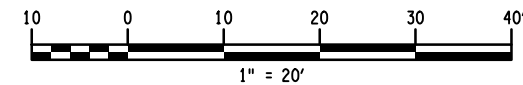
DESIGN SUPERVISOR J. PANGBURN
 JOB MANAGER C. GEROUX
 DESIGN L. SHORT
 CHECK A. BROWN
 DRAFTING K. DETRICK
 CHECK A. BROWN
 PROJECT MANAGER C. GEROUX



NOTES:

1. CONTRACTOR SHALL TEMPORARILY REMOVE ALL SNOW PLOW AND MILE POST MARKERS DURING CONSTRUCTION WHEN PROPOSED GUIDE RAIL IS INSTALLED. ALL SNOW PLOW AND MILE POST MARKERS REMOVED SHALL BE REINSTALLED AT THEIR ORIGINAL LOCATION UNLESS OTHERWISE NOTED IN THE PLANS.
2. CONTRACTOR SHALL REMOVE, STORE, AND RESET EXISTING WATER LEVEL GAUGE. COST FOR REMOVAL, STORAGE, AND RESETTING SHALL BE INCLUDED IN ITEM 646.24.
3. CONTRACTOR SHALL RESET EXISTING USGS BENCH MARK IN ACCORDANCE WITH THE NOTES ON DWG. MST-02. COST FOR RESETTING BENCH MARK SHALL BE INCLUDED IN SURVEY ITEM.

CURVE 1	
PC =	STA. 12+66.03
PT =	STA. 13+91.70
Δ =	5°55'34.95"
R =	1215.00'
Dc =	4°42'56.53"
L =	125.67'
T =	62.89'



PREPARED BY: JEFFREY W. PANGBURN, P.E.
 ALTERED BY: ON:



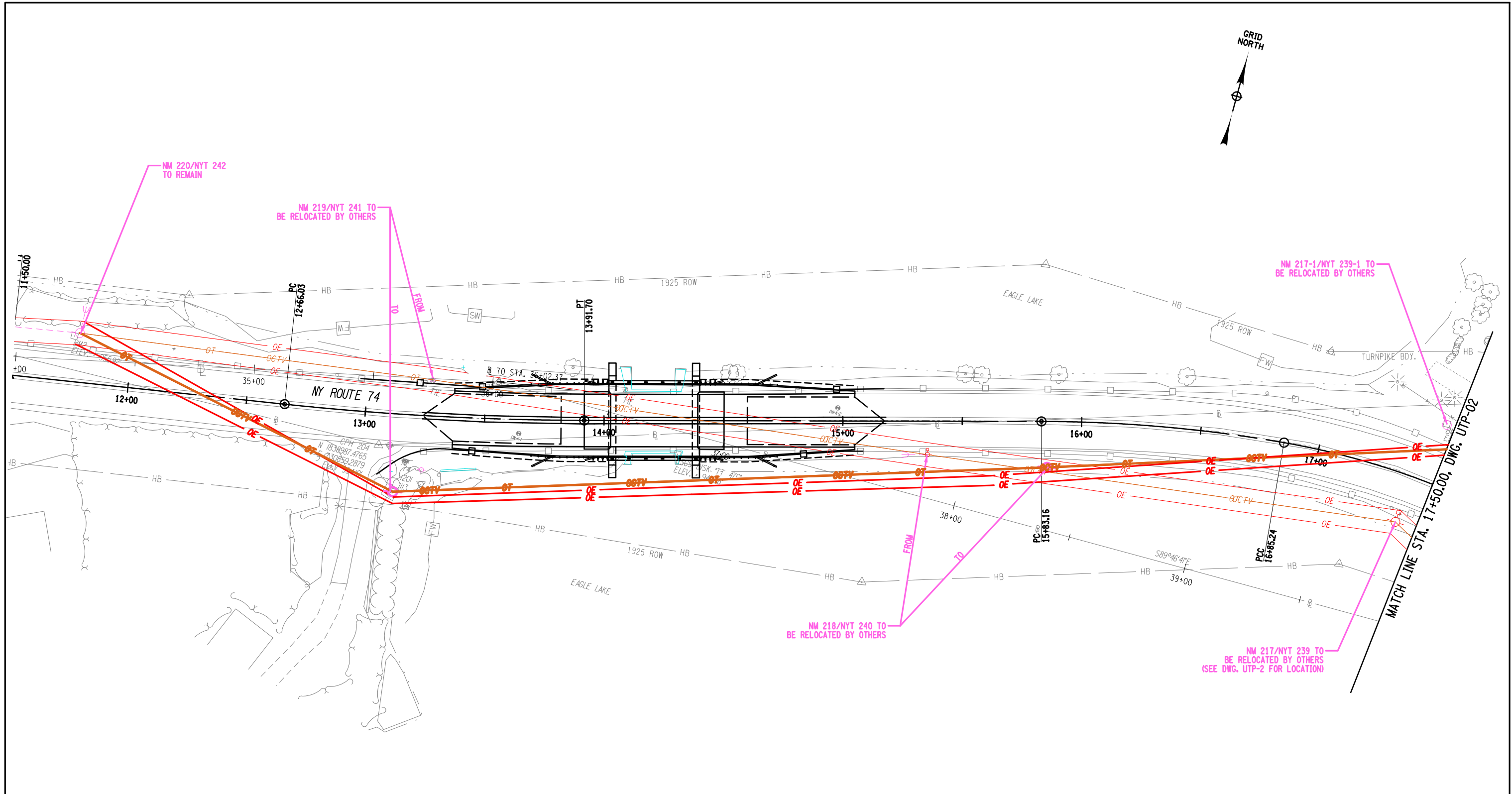
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	CULVERT REPLACEMENT	UTILITY QUALITY LEVEL "C"	1080770	C120086		
	SH 8230 SCHROON LAKE - TICONDEROGA, PART 2					
	TOWN OF TICONDEROGA					
	COUNTY: ESSEX COUNTY	REGION: 1			GENERAL PLAN	DRAWING NO. GNP-01 SHEET NO. 21

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.



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 PLOT = NYS DOT CME PDF plotter

DESIGN SUPERVISOR J. PANGBURN JOB MANAGER C. GEROUX DESIGN L. SHORT CHECK A. BROWN DRAFTING K. DETRICK CHECK A. BROWN PROJECT MANAGER C. GEROUX



PREPARED BY: JEFFREY W. PANGBURN, P.E. ALTERED BY: ON:



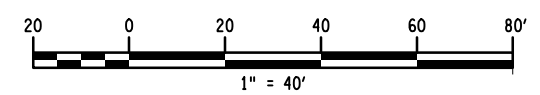
AS-BUILT REVISIONS
 DESCRIPTION OF ALTERATIONS:

NY ROUTE 74 OVER EAGLE LAKE CHANNEL
CULVERT REPLACEMENT
SH 8230 SCHROON LAKE - TICONDEROGA, PART 2
TOWN OF TICONDEROGA
COUNTY: ESSEX COUNTY REGION: 1

PIN 1110.02	BRIDGES 1080770	CULVERTS C120086
UTILITY QUALITY LEVEL "C"		

ALL DIMENSIONS IN FT UNLESS OTHERWISE NOTED
UTILITY PLAN

CONTRACT NUMBER D264836
DRAWING NO. UTP-01 SHEET NO. 22

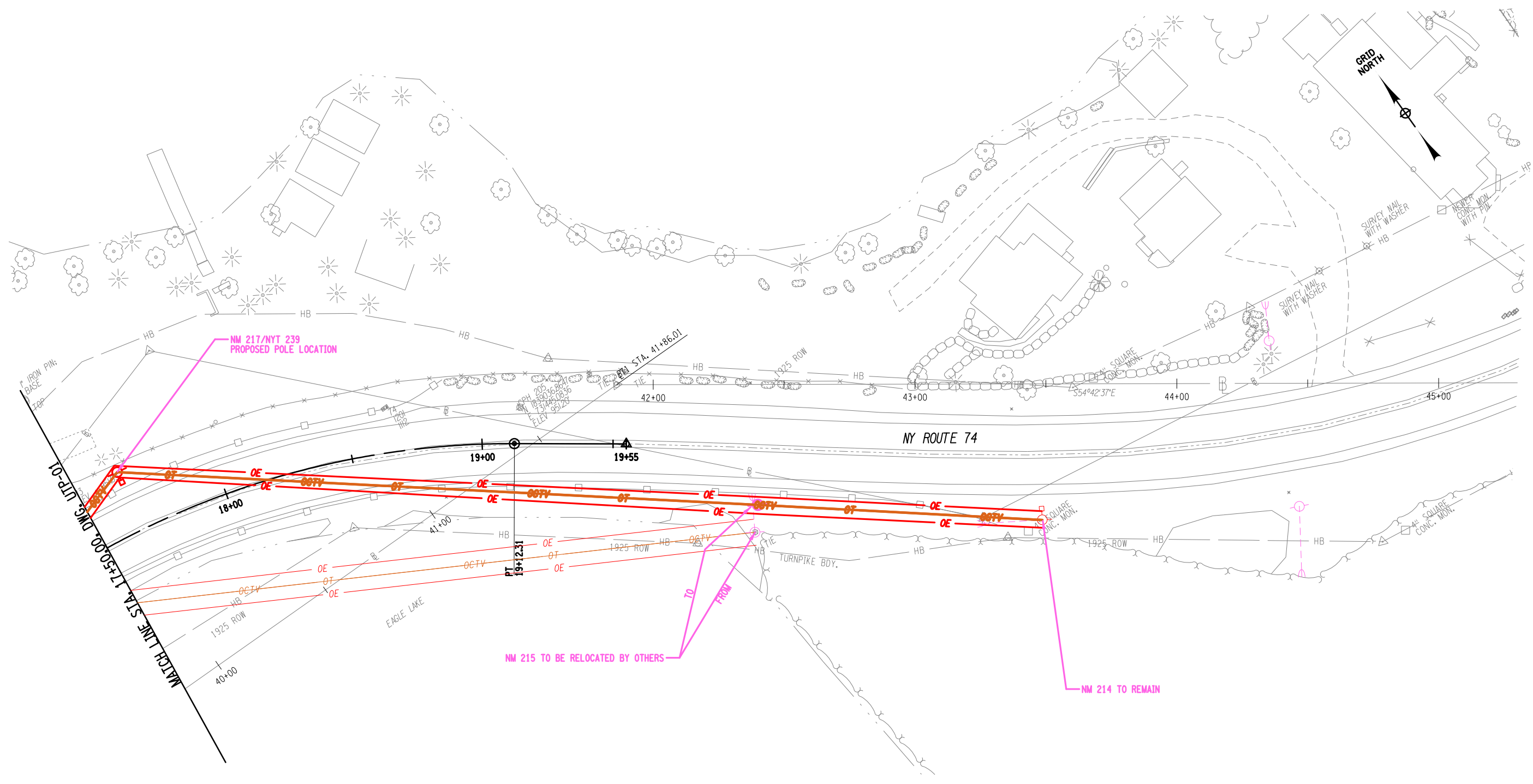


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DESIGN SUPERVISOR J. PANGBURN
 DESIGN L. SHORT
 CHECK A. BROWN
 DRAFTING K. DETRICK
 CHECK A. BROWN
 PROJECT MANAGER C. GEROUX



PREPARED BY: JEFFREY W. PANGBURN, P.E. ALTERED BY: ON:



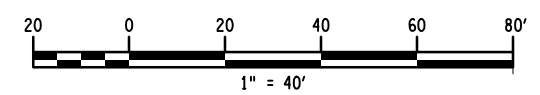
AS-BUILT REVISIONS
 DESCRIPTION OF ALTERATIONS:

NY ROUTE 74 OVER EAGLE LAKE CHANNEL
 CULVERT REPLACEMENT
 SH 8230 SCHROON LAKE - TICONDEROGA, PART 2
 TOWN OF TICONDEROGA
 COUNTY: ESSEX COUNTY REGION: 1

PIN 1110.02
 UTILITY QUALITY LEVEL "C"
 BRIDGES 1080770
 CULVERTS C120086

ALL DIMENSIONS IN FT UNLESS OTHERWISE NOTED
 UTILITY PLAN

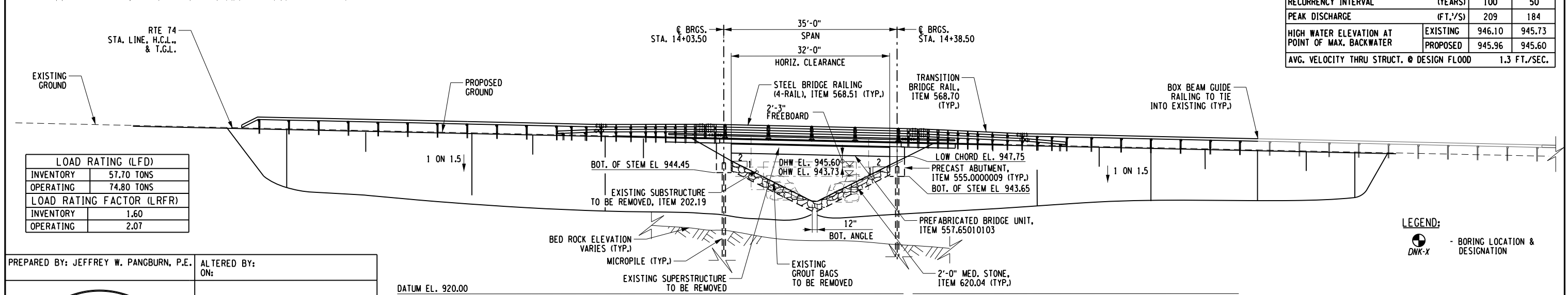
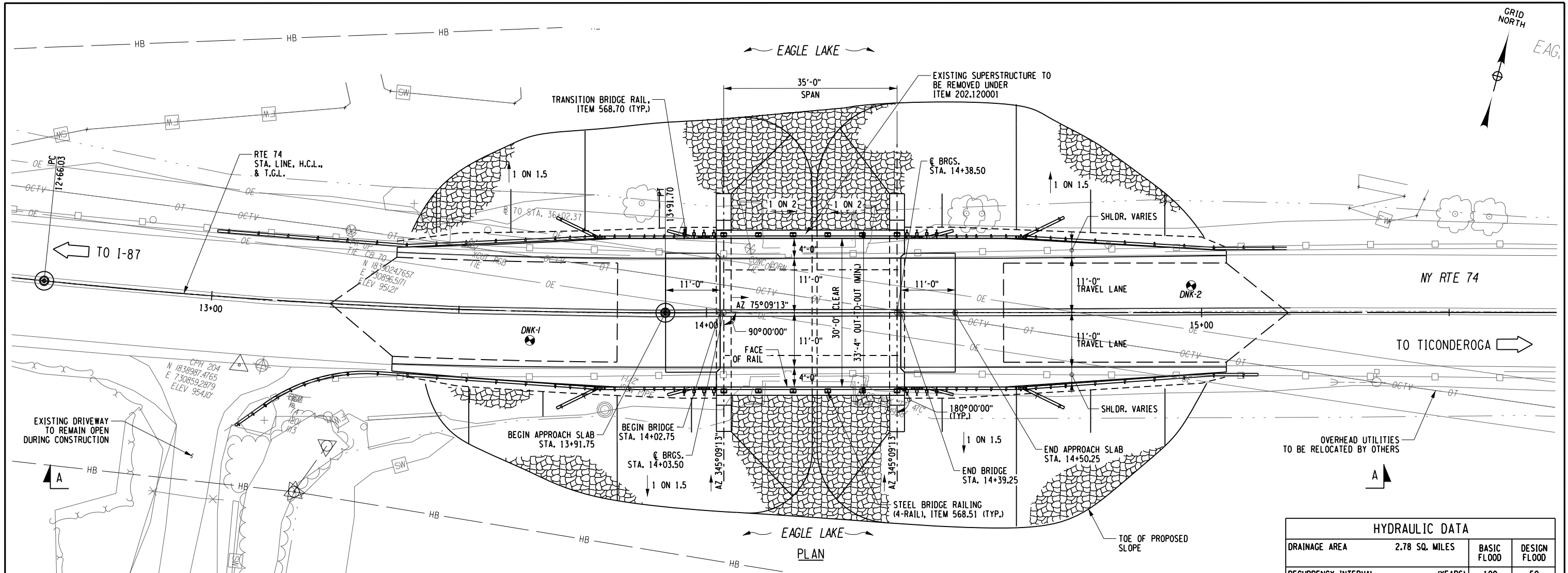
CONTRACT NUMBER
D264836
 DRAWING NO. UTP-02
 SHEET NO. 23



IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.



PROJECT MANAGER: C. GEROUX
 CHECK: A. BROWN
 DRAFTING: K. DETRICK
 CHECK: A. BROWN
 DESIGN: L. SHORT
 JOB MANAGER: C. GEROUX
 DESIGN SUPERVISOR: J. PANGBURN
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HYDRAULIC DATA			
DRAINAGE AREA	2.78 SQ. MILES	BASIC FLOOD	DESIGN FLOOD
RECURRENCE INTERVAL	(YEARS)	100	50
PEAK DISCHARGE	(FT. ³ /S)	209	184
HIGH WATER ELEVATION AT POINT OF MAX. BACKWATER	EXISTING	946.10	945.73
	PROPOSED	945.96	945.60
AVG. VELOCITY THRU STRUCT. @ DESIGN FLOOD	1.3 FT./SEC.		

LOAD RATING (LFD)	
INVENTORY	57.70 TONS
OPERATING	74.80 TONS
LOAD RATING FACTOR (LRFR)	
INVENTORY	1.60
OPERATING	2.07

PREPARED BY: JEFFREY W. PANGBURN, P.E. ALTERED BY: ON:



DATUM EL. 920.00

NOTE:
1. HIGH VOLTAGE ELECTRIC LINES ARE IN PROXIMITY TO THE BRIDGE

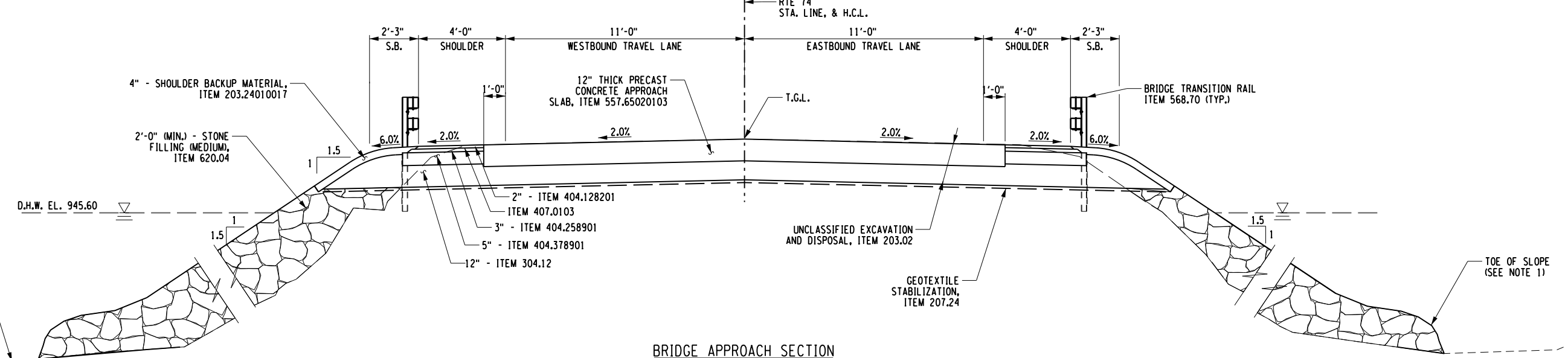
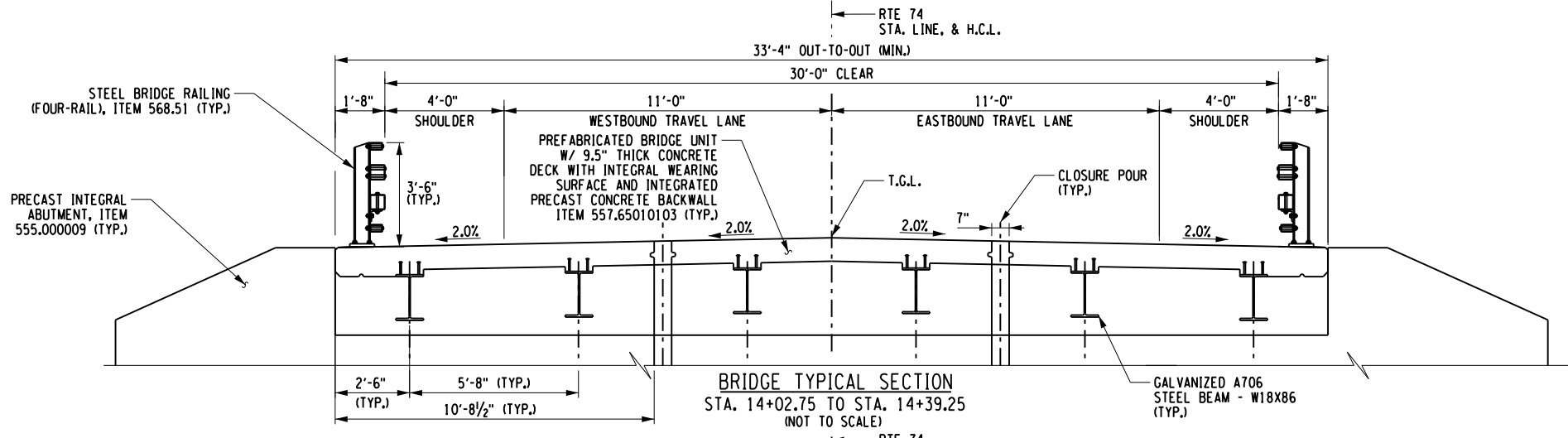
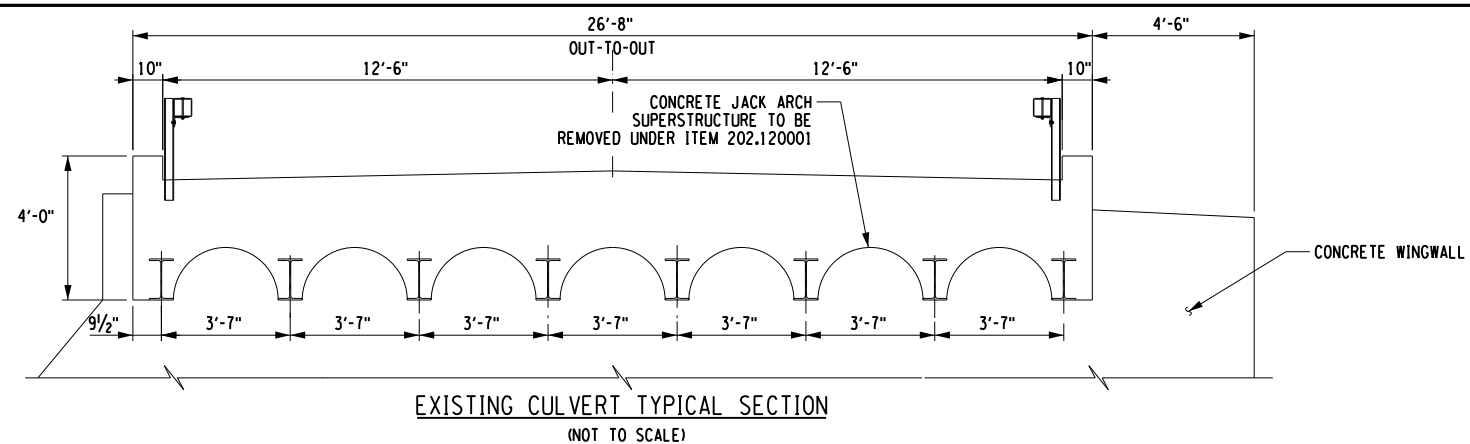
AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:	NY ROUTE 74 OVER EAGLE LAKE CHANNEL CULVERT REPLACEMENT SH 8230 SCHROON LAKE - TICONDEROGA, PART 2 TOWN OF TICONDEROGA COUNTY: ESSEX COUNTY REGION: 1	PIN 1110.02 UTILITY QUALITY LEVEL "C"	BRIDGES 1080770	CULVERTS C120086	ALL DIMENSIONS IN FT UNLESS OTHERWISE NOTED	CONTRACT NUMBER D264836
GENERAL PLAN AND ELEVATION					DRAWING NO. ST-1 SHEET NO. 24	

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 PLOT = NYS DOT CME PDF.plt:fg

PROJECT MANAGER C. GEROUX
 CHECK A. BROWN
 DRAFTING K. DETRICK
 CHECK A. BROWN
 DESIGN L. SHORT
 JOB MANAGER C. GEROUX
 DESIGN SUPERVISOR J. PANGBURN



PREPARED BY: JEFFREY W. PANGBURN, P.E. ALTERED BY: ON:



NOTE:
 1. THE WIDENING OF THE CAUSEWAY SHALL BE ACHIEVED BY PLACING STONE FILL INTO THE WATER UNTIL A STABLE TOE BASE IS ESTABLISHED.

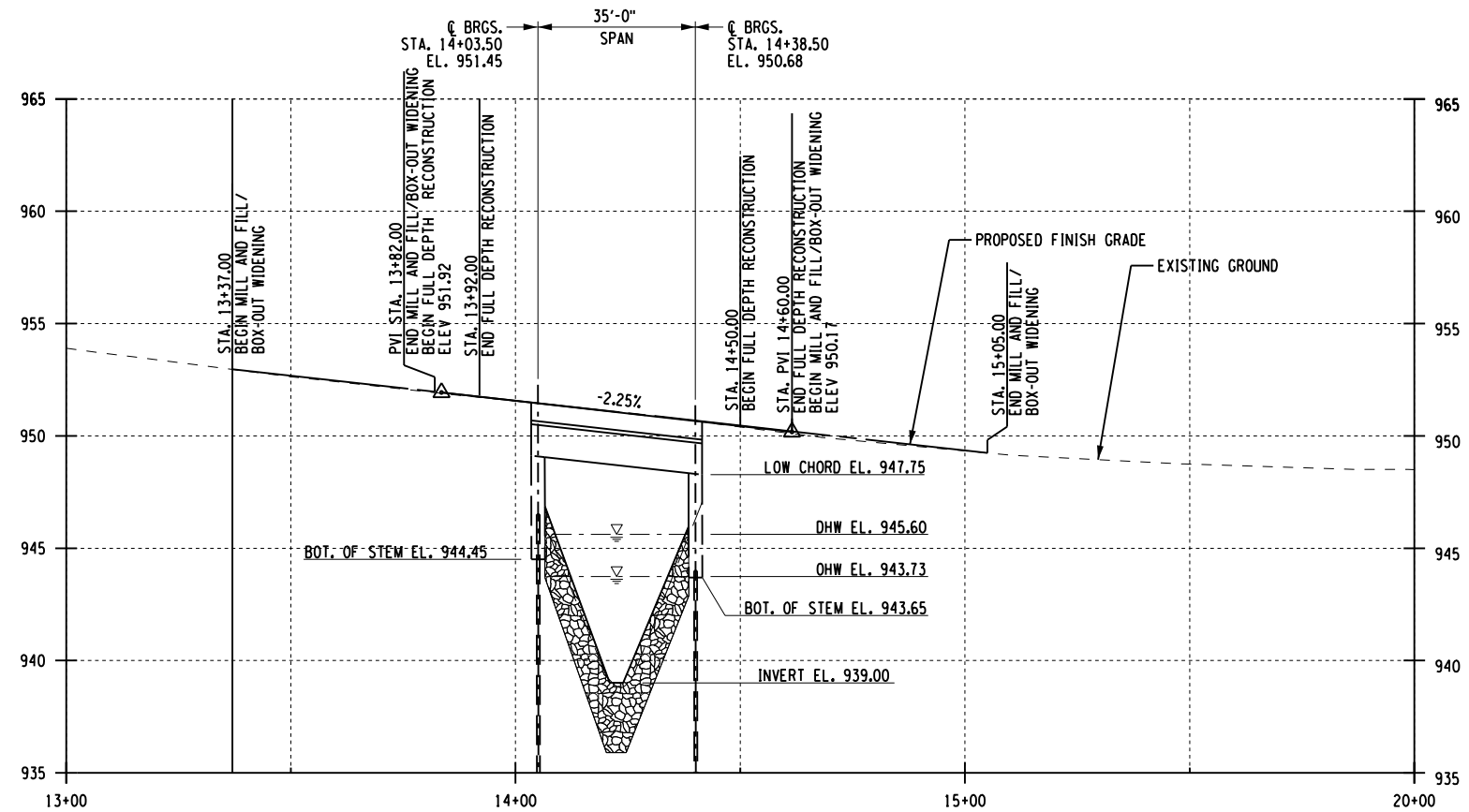
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	CULVERT REPLACEMENT	UTILITY QUALITY LEVEL "C"	1080770	C120086		D264836
	SH 8230 SCHROON LAKE - TICONDEROGA, PART 2				BRIDGE TYPICAL SECTIONS	DRAWING NO. ST-2
	TOWN OF TICONDEROGA					SHEET NO. 25
	COUNTY: ESSEX COUNTY	REGION: 1				

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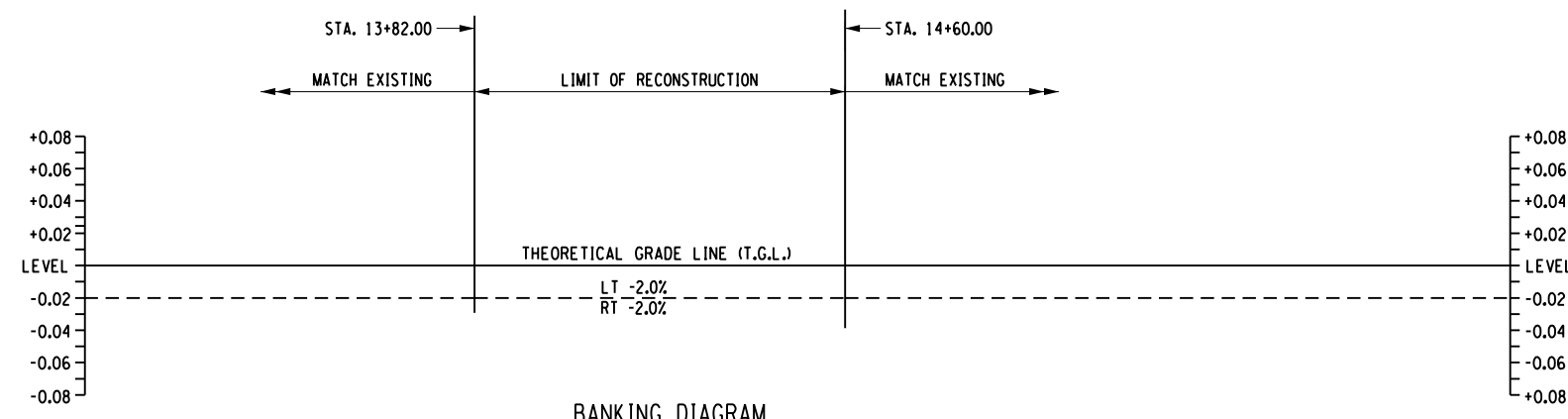


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DESIGN SUPERVISOR J. PANGBURN
 JOB MANAGER C. GEROUX
 DESIGN L. SHORT
 CHECK A. BROWN
 DRAFTING K. DETRICK
 CHECK A. BROWN
 PROJECT MANAGER C. GEROUX

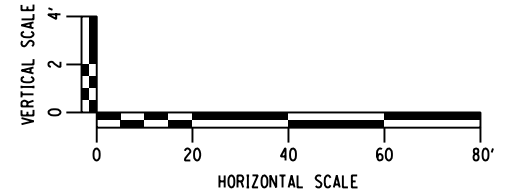


PROFILE - NYS ROUTE 74



BANKING DIAGRAM

LEGEND	
---	LEFT SIDE OF ROAD
---	RIGHT SIDE OF THE ROAD



PREPARED BY: JEFFREY W. PANGBURN, P.E. ALTERED BY: ON:



AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:	NY ROUTE 74 OVER EAGLE LAKE CHANNEL	PIN 1110.02	BRIDGES	CULVERTS	ALL DIMENSIONS IN FT UNLESS OTHERWISE NOTED	CONTRACT NUMBER
	CULVERT REPLACEMENT	UTILITY QUALITY LEVEL "C"	1080770	C120086		D264836
	SH 8230 SCHROON LAKE - TICONDEROGA, PART 2				PROFILE	DRAWING NO. ST-3
	TOWN OF TICONDEROGA					SHEET NO. 26
	COUNTY: ESSEX COUNTY REGION: 1					

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DESIGN SUPERVISOR J. PANGBURN
 DESIGN L. SHORT
 CHECK A. BROWN
 DRAFTING K. DETRICK
 CHECK A. BROWN
 PROJECT MANAGER C. GEROUX

GENERAL NOTES:

DESIGN SPECIFICATIONS: NYS DOT LRFD BRIDGE DESIGN SPECIFICATIONS WITH ALL PROVISIONS IN EFFECT AS OF MAY 2022 (FOR DESIGN PURPOSES, COMPRESSIVE STRENGTH OF CAST-IN-PLACE CONCRETE AT 28 DAYS: $f'_c = 3,000$ psi). CONCRETE COMPRESSIVE STRENGTH FOR PRECAST CONCRETE ELEMENTS SHALL BE IN ACCORDANCE WITH THE RESPECTIVE SPECIAL SPECIFICATIONS.

CONSTRUCTION AND MATERIALS SPECIFICATIONS: STANDARD SPECIFICATIONS, CONSTRUCTION AND MATERIALS, NEW YORK STATE DEPARTMENT OF TRANSPORTATION, OFFICE OF ENGINEERING.

THE BRIDGE SHALL BE MAINTAINED IN ACCORDANCE WITH THE GUIDELINES CONTAINED IN THE CURRENT VERSION OF THE AASHTO MAINTENANCE MANUAL FOR ROADWAYS AND BRIDGES.

THE LOAD RATINGS ARE IN ACCORDANCE WITH THE AASHTO MANUAL FOR BRIDGE EVALUATION.

DESIGN LIVE LOAD: AASHTO HL-93

DETAILS ON THE DRAWINGS LABELED AS "NOT TO SCALE" ARE INTENTIONALLY DRAWN NOT TO SCALE FOR VISUAL CLARITY. ALL OTHER DETAILS FOR WHICH NO SCALE IS SHOWN ARE DRAWN PROPORTIONAL AND ARE FULLY DIMENSIONED.

ALL SHOP DRAWINGS SUBMITTED FOR THIS PROJECT SHALL BE IN US CUSTOMARY UNITS.

THIS BRIDGE SHALL BE MAINTAINED IN ACCORDANCE WITH THE GUIDELINES CONTAINED IN THE CURRENT EDITION OF THE AASHTO MAINTENANCE MANUAL FOR ROADWAYS AND BRIDGES.

NO KNOWN ASBESTOS CONTAINING MATERIALS ARE BELIEVED TO EXIST AND/OR THE WORK PERFORMED UNDER THIS CONTRACT DOES NOT REQUIRE THE DISTURBANCE, DESTRUCTION OR REMOVAL OF ANY OF THESE MATERIALS. IT IS THE EXPRESS INTENT OF THIS CONTRACT THAT THESE MATERIALS ARE NOT TO BE DISTURBED IN ANY WAY. SHOULD THE CONTRACTOR DISTURB OR ENCOUNTER ANY SUCH MATERIALS, THE CONTRACTOR SHALL IMMEDIATELY STOP WORK AND NOTIFY THE ENGINEER. THE CONTRACTOR SHALL OBTAIN WRITTEN PERMISSION FROM THE ENGINEER BEFORE PROCEEDING.

HIGH VOLTAGE ELECTRICAL LINES ARE IN PROXIMITY TO THIS BRIDGE.

FOUNDATION NOTES:

WHERE PILES ARE TO BE PLACED THROUGH THE EMBANKMENT (6 INCH TOPSIZE), THE EMBANKMENT SHALL BE COMPACTED TO 95 PERCENT OF STANDARD PROCTOR MAXIMUM DENSITY.

HIGHWAY EMBANKMENT MATERIAL, ITEM 203.03 AND SELECT STRUCTURE FILL, ITEM 203.21, SHALL BE PLACED SIMULTANEOUSLY, IN CONTACT, ON BOTH SIDES OF THE VERTICAL PAYMENT LINE.

THE COST OF WATER USED FOR COMPACTION OF EMBANKMENT IN PLACE MATERIAL SHALL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 203.03 - EMBANKMENT IN PLACE.

THE COST OF WATER USED FOR COMPACTION OF SELECT STRUCTURE FILL SHALL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 203.21 - SELECT STRUCTURE FILL.

SUBSTRUCTURE NOTES:

THE COST OF ALL MATERIAL AT EACH CONSTRUCTION JOINT, CONTRACTION JOINT AND CONCRETE EXPANSION JOINT SHALL BE INCLUDED IN THE PRICES BID FOR THE VARIOUS ITEMS OF THE CONTRACT.

COFFERDAM AND HYDRAULIC NOTES:

SHOULD THE CONTRACTOR ELECT TO LAY BACK A PORTION OF THE EXISTING EARTH ADJACENT TO AN EXCAVATION REQUIRING A COFFERDAM, ANY REQUIRED EXTENSIONS OF THE COFFERDAM NECESSARY TO KEEP WATER FROM ENTERING THE EXCAVATION SHALL BE FURNISHED AND PLACED AT NO COST TO THE STATE.

WHERE A COFFERDAM IS USED, THE COST OF DEWATERING THE ENTIRE EXCAVATION, REGARDLESS OF THE SOURCE OF WATER, SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE COFFERDAM ITEM.

SHOULD FIELD CONDITIONS REQUIRE A CHANGE IN THE TYPE OF COFFERDAM SYSTEM CALLED FOR ON THE PLANS, THE ENGINEER SHALL SUBMIT THE CHANGES TO THE DCS FOR REVIEW AND APPROVAL.

IF MULTIPLE COFFERDAMS ARE REPLACED BY A SINGLE SYSTEM, AS PERMITTED BY THE REGIONAL HYDRAULICS ENGINEER, PAYMENT WILL BE BASED ON ALL OF THE APPLICABLE COFFERDAM ITEMS INDICATED ON THE PLANS.

DEWATER THE COFFERDAM BY PUMPING THE WATER INTO SEDIMENT FILTER BAGS AS SHOWN ON THE PLANS AND/OR APPROVED BY THE ENGINEER. TEMPORARY SOIL EROSION AND WATER POLLUTION CONTROL, SUCH AS STRAW BALES OR APPROVED EQUAL, MAY BE REQUIRED AS DETERMINED BY THE ENGINEER. NO SETTLEMENT BASIN SHALL BE CONSTRUCTED.

ORDINARY HIGH WATER IS ESTIMATED TO BE 943.73. THIS IS DEFINED AS THE WATER SURFACE ELEVATION FOR THE MEAN ANNUAL FLOOD, WHICH IS THE FLOOD THAT HAS A RECURRENCE INTERVAL OF 2.33 YEARS.

ORDINARY WATER IS ESTIMATED TO BE 943.00. THIS IS DEFINED AS THE HIGHEST SURFACE WATER ELEVATION LIKELY TO BE ENCOUNTERED DURING ONE CONSTRUCTION SEASON (OTHER THAN MAJOR FLOODS). IT IS ALWAYS LESS THAN THE ORDINARY HIGH WATER ELEVATION AND IT IS USUALLY AN OBSERVED ELEVATION RATHER THAN A COMPUTED ONE.

LOW WATER IS ESTIMATED TO BE 942.75. THIS WATER ELEVATION IS THE NORMAL LOW WATER ELEVATION PREVALENT DURING ONE CONSTRUCTION SEASON FOR MORE THAN 25% OF THE TIME. IT IS AN OBSERVED ELEVATION RATHER THAN A COMPUTED ONE.

SUPERSTRUCTURE NOTES:

THE COST OF CLEANING THIS STEEL IN THE FABRICATION SHOP AND THE FIELD SHALL BE INCLUDED IN THE UNIT PRICES BID FOR THE VARIOUS STRUCTURAL STEEL ITEMS.

DIAPHRAGMS SHALL BE FABRICATED TO FIT GIRDERS ERECTED WITH THEIR WEBS PLUMB UNDER FULL DEAD LOAD CONDITIONS, ALSO KNOWN AS TOTAL DEAD LOAD FIT (TDLF).

GALVANIZING NOTES:

ALL STRUCTURAL STEEL SHALL BE HOT DIPPED GALVANIZED THE HOT-DIP GALVANIZING SHALL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 557.65010103 - PREFABRICATED MODULAR DECK BEAM.

ALL BOLTS, NUTS, AND WASHERS SHALL BE GALVANIZED IN ACCORDANCE WITH THE NEW YORK STATE STEEL CONSTRUCTION MANUAL.

DRILLED HOLES SHALL BE CLEANED OF EXCESS GALVANIZED COATING THAT PREVENTS PROPER BOLT INSTALLATION.

STUD SHEAR CONNECTORS SHALL BE INSTALLED PRIOR TO GALVANIZING.

REASONABLE ACCOMMODATIONS FOR THE PREVENTION OF WET STORAGE STAINING (WHITE RUST) OF HOT-DIPPED GALVANIZED (HDG) MATERIALS SHALL BE PROVIDED AT ALL TIMES. STORAGE OF HDG MATERIALS OUTDOORS SHOULD BE AVOIDED. STORAGE (OR SHIPPING) OF HDG MATERIALS IN CONTACT WITH ONE ANOTHER SHALL BE AVOIDED. IF OUTDOOR STORAGE IS UNAVOIDABLE, EXAMPLES OF REASONABLE ACCOMMODATIONS ARE AS FOLLOWS: STORE MATERIALS OFF OF THE GROUND AWAY FROM ALL VEGETATION, USE NON-RESINOUS WOODEN SPACERS TO ALLOW VENTILATION AND AVOID MOISTURE BUILD UP, INCLINE MEMBERS TO ALLOW DRAINAGE. EXAMPLES OF NON-RESINOUS WOODS ARE: POPLAR, ASH AND SPRUCE. WHITE RUST THAT IS DETERMINED TO BE DETRIMENTAL TO THE INTENDED USE OF THE MEMBER OR HAVE A NEGATIVE VISUAL IMPACT ON THE STRUCTURE SHALL BE REPAIRED IN ACCORDANCE WITH THE NYS STEEL CONSTRUCTION MANUAL. WHITE RUST THAT IS DETERMINED TO BE CAUSED BY IMPROPER STORAGE OR SHIPPING OF HDG MATERIALS SHALL BE REPAIRED AT NO COST TO THE STATE.

STEEL ERECTION NOTES:

THE CONTRACTOR SHALL PROVIDE FOR THE STABILITY OF STRUCTURAL STEEL DURING ALL PHASES OF ERECTION AND CONSTRUCTION, AS PROVIDED IN SECTION 2, SUBSECTION 204 OF THE NEW YORK STATE STEEL CONSTRUCTION MANUAL (SCM). THE MEANS AND METHODS USED BY THE CONTRACTOR SHALL BE DOCUMENTED ON THE ERECTION DRAWINGS AND INCLUDED IN THE SUPPORTING STABILITY CALCULATIONS. THE ERECTION DRAWINGS AND STABILITY CALCULATIONS SHALL BE SEALED BY A REGISTERED NEW YORK STATE PROFESSIONAL ENGINEER AND SUBMITTED TO THE DCS IN ACCORDANCE WITH THE SCM.

THE DESIGN OF THIS STRUCTURE ASSUMES THAT THE STRUCTURAL STEEL IS COMPLETELY ERECTED BEFORE IT IS ALLOWED TO DEFLECT UNDER ITS OWN DEAD LOAD. DEFLECTIONS INCURRED DURING THE VARIOUS STAGES OF THE ERECTION METHOD ARE NOT CONSIDERED. THEREFORE, THE ACTUAL ERECTION METHODS AND SEQUENCES EMPLOYED BY THE CONTRACTOR MAY HAVE A SUBSTANTIAL EFFECT ON THE FINAL STEEL PROFILE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR TAKING ALL NECESSARY COMPENSATORY ACTION TO ENSURE THAT THE FINAL ALIGNMENT AND PROFILE OF THE ERECTED STEEL CONFORMS TO SUBSECTION 1213, 1214, AND 1215 OF THE NEW YORK STATE STEEL CONSTRUCTION MANUAL (SCM). ANY CORRECTIVE WORK NECESSARY TO RE-POSITION PREVIOUSLY ERECTED STEEL TO ACHIEVE ACCEPTABLE ALIGNMENT AND PROFILE MUST BE APPROVED BY THE DCS, AND SHALL BE PERFORMED AT NO ADDITIONAL COST TO THE STATE.

ADDITIONAL STEEL CONSTRUCTION NOTES:

DIMENSIONS FOR THICKNESSES OF STEEL ROLLED ANGLE SHAPES AND STRUCTURAL TUBING ARE SHOWN ACCORDING TO THE AISC MANUAL.

SUPERSTRUCTURE SLAB NOTES:

THE PROVISIONS OF THE CURRENT SPECIFICATIONS FOR SUPERSTRUCTURE SLABS ALLOW THE OPTION OF 3 FORMING SYSTEMS FOR THE UNDERSIDE OF THE SLABS.

ON THIS BRIDGE, ONLY THE FOLLOWING OPTION(S) WILL BE PERMITTED: REMOVABLE WOODEN FORMS.

SHOP DRAWING SUBMITTALS ARE REQUIRED FOR THE FOLLOWING BRIDGE RAIL/TRANSITION ITEMS: 568.51; 568.70

TOP SURFACES OF THE NEW BRIDGE DECK SHALL BE SEALED TO THE LIMITS SHOWN IN THE PLANS IN ACCORDANCE WITH ITEM 559.01 PROTECTIVE SEALING OF STRUCTURAL CONCRETE ON NEW BRIDGE DECKS AND BRIDGE DECK OVERLAYS.

PREPARED BY: JEFFREY W. PANGBURN, P.E. ALTERED BY: ON:



AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:	NY ROUTE 74 OVER EAGLE LAKE CHANNEL CULVERT REPLACEMENT SH 8230 SCHROON LAKE - TICONDEROGA, PART 2 TOWN OF TICONDEROGA COUNTY: ESSEX COUNTY REGION: 1
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PIN 1110.02 UTILITY QUALITY LEVEL "C"	BRIDGES 1080770	CULVERTS C120086
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ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED	CONTRACT NUMBER D264836
GENERAL NOTES - 1	DRAWING NO. ST-5 SHEET NO. 28

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 DATE/TIME = 4/27/2022 2:52:24 PM
 USER = KDetrick
 PLOT = NYSDOT.CME.PDF.plt:fg

DESIGN SUPERVISOR J. PANGBURN
 JOB MANAGER C. GEROUX
 DESIGN L. SHORT
 CHECK A. BROWN
 DRAFTING K. DETRICK
 CHECK A. BROWN
 PROJECT MANAGER C. GEROUX

REMOVAL NOTES:

EXISTING SUBSTRUCTURES SHALL BE REMOVED WITHIN THE PAY LIMITS SHOWN IN THE PLANS UNDER ITEM 202.19 - REMOVAL OF SUBSTRUCTURES.

EXISTING SUPERSTRUCTURE SHALL BE REMOVED UNDER ITEM 202.120001 - REMOVAL OF EXISTING SUPERSTRUCTURES.

THE REQUIREMENTS OF §202-3.01 GENERAL AND SAFETY REQUIREMENTS. A REMOVAL PLAN SHALL BE SUBMITTED TO THE ENGINEER FIFTEEN (15) DAYS PRIOR TO BEGINNING THE DEMOLITION. THE REQUIREMENT THAT IT BE SIGNED BY A LICENSED AND REGISTERED PROFESSIONAL ENGINEER IS WAIVED.

RECORD PLANS FOR THIS STRUCTURE ARE NOT AVAILABLE.

LIMITS AND METHODS FOR REMOVAL OF PAINT AT LOCATIONS OF FASTENER REMOVAL OR FLAME CUTTING SHALL BE AS DESCRIBED IN §202-3.05 AND §574 OF THE STANDARD SPECIFICATIONS. THE COST OF PAINT REMOVAL SHALL BE INCLUDED IN THE LUMP SUM PRICE(S) BID FOR THE SUPERSTRUCTURE REMOVAL ITEM(S) (OR THE UNIT PRICE BID FOR THE SUBSTRUCTURE REMOVAL ITEM). PAINT WASTE NOT COLLECTED BY VACUUM METHODS SHALL BE COLLECTED USING THE ENVIRONMENTAL GROUND AND/OR WATERWAY PROTECTION ITEM(S). WASTE SHALL BE DISPOSED OF USING THE TREATMENT AND DISPOSAL OF PAINT REMOVAL WASTE ITEM.

LOOSE AND/OR PEELING PAINT ON STEEL SURFACES MAY BECOME DISLODGED DURING REMOVAL OPERATIONS OR DURING TRANSPORTATION FROM THE SITE UNLESS APPROPRIATE MEASURES ARE TAKEN. THE CONTRACTOR SHALL FORMULATE AND SUBMIT A METHOD OF REMEDIATING THE CONDITION FOR APPROVAL BY THE ENGINEER. WORKER LEAD PROTECTION IN ACCORDANCE WITH OSHA 1926.62 MUST BE SATISFIED. ALTERNATIVES COULD INCLUDE TRANSPORTING AFFECTED MEMBERS IN CLOSED TRUCKS, WRAPPING AFFECTED MEMBERS PRIOR TO REMOVAL, ENCAPSULATING THE LOOSE PAINT OR REMOVAL OF LOOSE PAINT PRIOR TO DISMANTLING OPERATIONS. THE COST OF REMEDIATING THIS CONDITION SHALL BE INCLUDED IN THE LUMP SUM PRICE(S) BID FOR THE SUPERSTRUCTURE REMOVAL ITEM(S) (OR THE UNIT PRICE BID FOR THE SUBSTRUCTURE REMOVAL ITEM.) THE USE OF ENVIRONMENTAL GROUND AND/OR WATERWAY PROTECTION ITEMS WILL BE REQUIRED. DEPENDING ON THE ALTERNATIVE CHOSEN, THE TREATMENT AND DISPOSAL OF PAINT REMOVAL WASTE ITEM MAY BE REQUIRED. BECAUSE OF THE ABOVE-MENTIONED CONDITION, THE CONTRACTOR SHOULD EXAMINE THE CONDITION OF THE STRUCTURE'S PAINT PRIOR TO SUBMITTING A BID.

RECONSTRUCTION NOTES:

DUE TO THE NATURE OF RECONSTRUCTION PROJECTS, THE EXACT EXTENT OF RECONSTRUCTION WORK CANNOT BE ACCURATELY DETERMINED PRIOR TO THE COMMENCEMENT OF WORK. THESE CONTRACT DOCUMENTS HAVE BEEN PREPARED BASED ON FIELD INSPECTION AND OTHER INFORMATION AVAILABLE AT THE TIME. ACTUAL FIELD CONDITIONS MAY REQUIRE MODIFICATIONS TO CONSTRUCTION DETAILS AND WORK QUANTITIES. THE CONTRACTOR SHALL PERFORM THE WORK IN ACCORDANCE WITH FIELD CONDITIONS.

THE CONTRACTOR SHALL VERIFY DIMENSIONS NECESSARY FOR THE PROPER FIT OF STEEL PIECES PRIOR TO THE FABRICATION OF THE STEEL. THE COST OF FIELD VERIFYING DIMENSIONS SHALL BE INCLUDED IN THE PRICE BID FOR STRUCTURAL STEEL ITEMS.

THE CONTRACTOR SHALL PERFORM ALL WORK WITH CARE SO THAT ANY MATERIALS WHICH ARE TO REMAIN IN PLACE, OR WHICH ARE TO REMAIN THE PROPERTY OF THE STATE, WILL NOT BE DAMAGED. IF THE CONTRACTOR DAMAGES ANY MATERIALS WHICH ARE TO REMAIN IN PLACE OR WHICH ARE TO REMAIN THE PROPERTY OF THE STATE, THE DAMAGED MATERIALS SHALL BE REPAIRED OR REPLACED IN A MANNER SATISFACTORY TO THE ENGINEER AT THE EXPENSE OF THE CONTRACTOR.

WHENEVER ITEMS IN THE CONTRACT REQUIRE MATERIALS TO BE REMOVED AND DISPOSED OF, THE COST OF SUPPLYING A DISPOSAL AREA AND TRANSPORTATION TO THAT AREA SHALL BE INCLUDED IN THE UNIT PRICES BID FOR THOSE ITEMS.

DURING REMOVAL OPERATIONS, THE CONTRACTOR SHALL NOT DROP WASTE CONCRETE, DEBRIS, AND OTHER MATERIAL TO THE AREA BELOW THE BRIDGE EXCEPT WHERE THE PLANS SPECIFICALLY PERMIT THE DROPPING OF MATERIAL. PLATFORMS, NETS, SCREENS OR OTHER PROTECTIVE DEVICES SHALL BE USED TO CATCH THE MATERIAL. IF ADEQUATE PROTECTIVE DEVICES ARE NOT BEING EMPLOYED, THE WORK WILL BE STOPPED UNTIL ADEQUATE PROTECTION IS PROVIDED.

ALL MATERIAL FALLING ON THE AREA BELOW AND ADJACENT TO THE BRIDGE SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AT NO COST TO THE STATE.

THE COST OF FURNISHING, INSTALLING, MAINTAINING, REMOVING AND DISPOSING OF ALL PLATFORMS, NETS, SCREENS OR OTHER PROTECTIVE DEVICES SHALL BE INCLUDED IN THE PRICES BID FOR THE APPROPRIATE ITEMS OF THE CONTRACT.

MISCELLANEOUS NOTES:

DURING THE COURSE OF CONSTRUCTION, THE CONTRACTOR SHALL CONDUCT OPERATIONS IN SUCH A MANNER AS TO PREVENT OR REDUCE TO A MINIMUM ANY DAMAGE TO ANY STREAM FROM POLLUTION BY DEBRIS, SEDIMENT, OR OTHER FOREIGN MATERIAL, OR FROM MANIPULATION OF EQUIPMENT AND/OR MATERIALS IN OR NEAR SUCH STREAMS. THE CONTRACTOR SHALL NOT RETURN DIRECTLY TO A STREAM ANY WATER WHICH HAS BEEN USED FOR WASH PURPOSES OR OTHER SIMILAR OPERATIONS WHICH CAUSE THIS WATER TO BECOME POLLUTED WITH SAND, SILT, CEMENT, OIL, OR OTHER IMPURITIES. IF THE CONTRACTOR USES WATER FROM A STREAM, THE CONTRACTOR SHALL CONSTRUCT AN INTAKE OR TEMPORARY DAM REQUIRED TO PROTECT AND MAINTAIN WATER RIGHTS AND TO SUSTAIN FISH LIFE DOWNSTREAM.

PREFABRICATED MODULAR DECK BEAM ELEMENT NOTES:

CONCRETE IN THE DECK SLAB SHALL HAVE A MINIMUM COMPRESSIVE STRENGTH OF 5000 PSI AT 28 DAYS. THE UNITS SHALL NOT BE HANDLED UNTIL CONCRETE STRENGTH REACHES A MINIMUM OF 3,000 PSI.

STRUCTURAL STEEL SHALL BE ASTM A709, GRADE 50.

HIGH STRENGTH BOLTS USED IN DIAPHRAGM CONNECTIONS SHALL BE ASTM F3125, GRADE A325. (USE TYPE 1 FOR PAINTED STEEL, TYPE 3 FOR WEATHERING STEEL).

TO ENSURE FULL AND EVEN BEARING BETWEEN BOTTOM OF BEAMS AND MASONRY PLATES, THE BOTTOM SURFACES OF BEAMS IN THE BEARING AREAS SHALL, WITHIN EACH PANEL, BE FABRICATED TO BE TRULY IN ONE PLANE.

ALL REINFORCEMENT SHALL HAVE A COVER OF 2 INCHES (TO BOTTOM OF LONGITUDINAL GROOVES) UNLESS SHOWN OTHERWISE. THE TOP BARS IN THE DECK AND APPROACH SLAB SHALL BE EPOXY COATED. NO CHAIRS, BOLSTERS OR OTHER SUPPORT DEVICES SHALL BE EXPOSED ON THE SURFACE THAT WILL BE THE TOP OF DECK IN FIELD.


THE MODULAR DECK BEAMS MAY BE CONSTRUCTED WITHOUT DIAPHRAGMS. HOWEVER, PRIOR TO TRANSPORTATION TO THE BRIDGE SITE, ALL DIAPHRAGMS INTEGRAL TO ANY ONE UNIT SHALL BE INSTALLED.

THIS IS A NON-MATCH CAST SEGMENTAL CONSTRUCTION. ALL PROVISIONS OF 'SECTION 2.3 INSTALLATION DRAWINGS AND SUPPORTING DOCUMENTS' OF THE PRECAST CONCRETE CONSTRUCTION MANUAL (PCCM), EXCEPT PROVISIONS RELATED TO POST-TENSIONING, SHALL APPLY.

PROCEDURE FOR PREPARING BLOCKOUT SURFACES, PLACING AND CURING BACKFILL, ETC. SHALL BE SHOWN ON INSTALLATION DRAWINGS.

WATER BODY PROTECTION NOTES:

THE CONTRACTOR SHALL ACCOMPLISH IN-WATER WORK DURING THE PERIOD BETWEEN MAY 1 AND SEPTEMBER 30. COFFERDAMS IN THE WATERWAY CHANNEL AND/OR WATER DIVERSIONS OF THE CHANNEL SHALL NOT BE ALLOWED PRIOR TO JUNE 15 AND AFTER SEPTEMBER 30 WITHOUT PRIOR WRITTEN APPROVAL FROM THE NEW YORK STATE DEPARTMENT OF ENVIRONMENTAL CONSERVATION AND NEW YORK STATE DEPARTMENT OF TRANSPORTATION. COFFERDAMS AND WATER DIVERSIONS SHALL BE SIZED WITH REGARD TO THE SEASONAL FLOW OF THE WATER BODY EXPECTED FOR THE TIME THEY ARE TO BE IN USE.

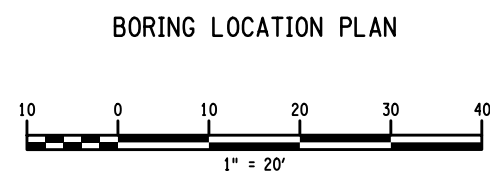
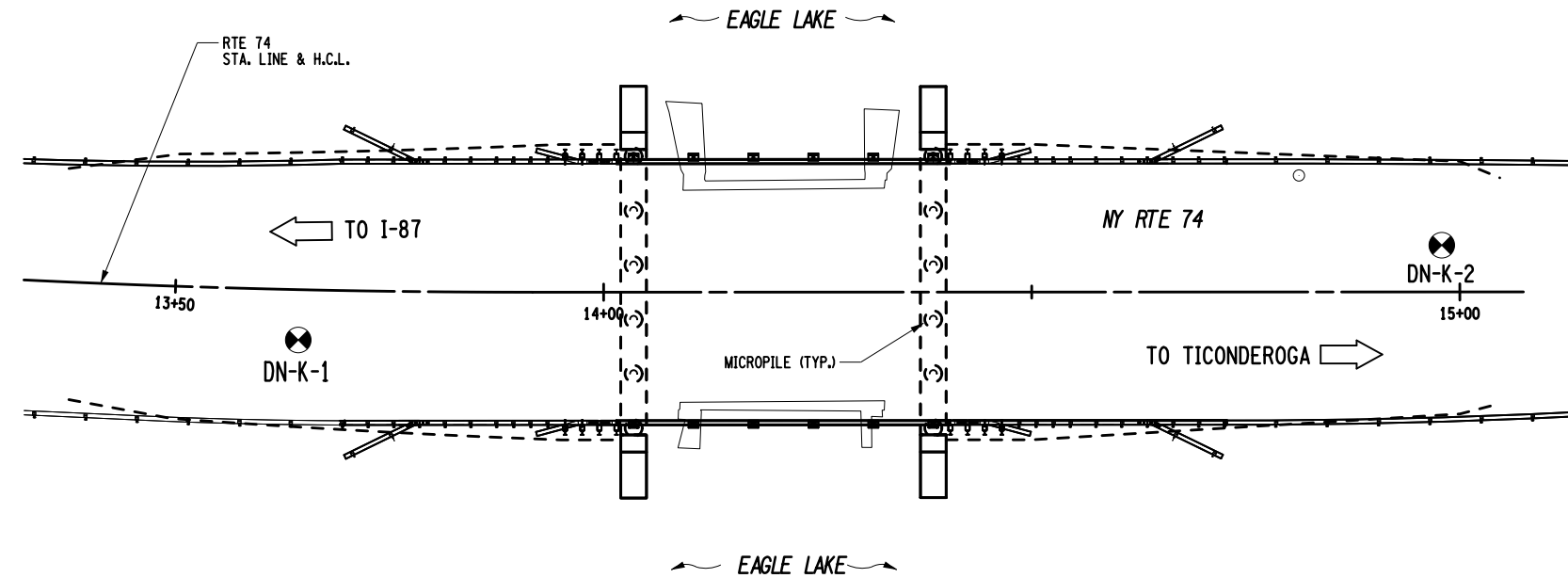
PREPARED BY: JEFFREY W. PANGBURN, P.E.	ALTERED BY: ON:																													
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	<table border="1"> <tr> <td>NY ROUTE 74 OVER EAGLE LAKE CHANNEL</td> <td>PIN 1110.02</td> <td>BRIDGES</td> <td>CULVERTS</td> <td rowspan="4">ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED</td> <td rowspan="4">CONTRACT NUMBER D264836</td> </tr> <tr> <td>CULVERT REPLACEMENT</td> <td>UTILITY QUALITY LEVEL "C"</td> <td>1080770</td> <td>C120086</td> </tr> <tr> <td>SH 8230 SCHROON LAKE - TICONDEROGA, PART 2</td> <td></td> <td></td> <td></td> </tr> <tr> <td>TOWN OF TICONDEROGA</td> <td></td> <td></td> <td></td> </tr> <tr> <td>COUNTY: ESSEX COUNTY</td> <td>REGION: 1</td> <td></td> <td></td> <td>GENERAL NOTES - 2</td> <td>DRAWING NO. ST-6</td> </tr> <tr> <td></td> <td></td> <td></td> <td></td> <td></td> <td>SHEET NO. 29</td> </tr> </table>	NY ROUTE 74 OVER EAGLE LAKE CHANNEL	PIN 1110.02	BRIDGES	CULVERTS	ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED	CONTRACT NUMBER D264836	CULVERT REPLACEMENT	UTILITY QUALITY LEVEL "C"	1080770	C120086	SH 8230 SCHROON LAKE - TICONDEROGA, PART 2				TOWN OF TICONDEROGA				COUNTY: ESSEX COUNTY	REGION: 1			GENERAL NOTES - 2	DRAWING NO. ST-6					
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 DATE/TIME = 21 APR 2022
 USER = sjohnson4

DESIGN SUPERVISOR J.J.D. 4/21/22
 JOB MANAGER S.L.B.
 DESIGN S.L.J.
 DRAFTING S.L.J.
 CHECK
 PROJECT MANAGER



GENERAL NOTES

- 1.) SOUND ENGINEERING JUDGMENT WAS EXERCISED IN PREPARING THE SUBSURFACE INFORMATION PRESENTED HERE ON. THIS INFORMATION WAS PREPARED AND IS INTENDED FOR STATE DESIGN AND ESTIMATE PURPOSES ONLY. ITS PRESENTATION ON THE PLANS OR ELSEWHERE IS FOR THE PURPOSE OF PROVIDING INTENDED USERS WITH ACCESS TO THE SAME INFORMATION AVAILABLE TO THE STATE. THIS IS NOT INTENDED AS A SUBSTITUTE FOR PERSONAL INVESTIGATION, INDEPENDENT INTERPRETATIONS OR JUDGEMENT OF THE CONTRACTOR.
- 2.) GENERAL SOIL AND ROCK (WHERE ENCOUNTERED) STRATUM DESCRIPTIONS AND INDICATED BOUNDARIES ARE BASED ON AN ENGINEERING INTERPRETATION OF ALL AVAILABLE SUBSURFACE INFORMATION BY THE GEOTECHNICAL ENGINEERING BUREAU AND MAY NOT NECESSARILY REFLECT THE ACTUAL VARIATION IN SUBSURFACE CONDITIONS BETWEEN BORINGS AND SAMPLES. DETAILED DATA AND FIELD INTERPRETATIONS OF CONDITIONS ENCOUNTERED IN INDIVIDUAL BORINGS ARE SHOWN ON THE SUBSURFACE EXPLORATION LOGS.
- 3.) THE OBSERVED WATER LEVELS AND/OR CONDITIONS INDICATED ON THE SUBSURFACE PROFILES ARE AS RECORDED AT THE TIME OF EXPLORATION. ACTUAL WATER LEVELS MAY DIFFER FROM THE OBSERVED WATER LEVEL BECAUSE OF LIMITATIONS IN THE NUMBER AND DURATION OF OBSERVATIONS AND WILL VARY WITH CHANGES IN CLIMATE AND RAINFALL.
- 4.) ALL STRUCTURE DETAILS AND FOOTING ELEVATIONS SHOWN HEREON ARE FOR ILLUSTRATIVE PURPOSES ONLY AND MAY NOT BE INDICATIVE OF THE FINAL DESIGN CONDITIONS SHOWN ON THE CONTRACT PLANS.

LEGEND

THE FOLLOWING TABLES SUMMARIZE THE DESCRIPTIVE INFORMATION USED ON THIS PROFILE.

DENSITY (NON-PLASTIC SOILS)	NO. OF BLOWS PER FOOT OF PENETRATION OF A 2 INCH O.D. (1-3/8 INCH I.D.) SAMPLER USING A 140 LB. HAMMER, 30 INCH FALL.
Very Loose	0-4
Loose	5-10
Medium Compact	11-24
Compact	25-50
Very Compact	over 50

CONSISTENCY (PLASTIC SOILS)	
Very Soft	0-1
Soft	2-4
Medium Stiff	5-8
Stiff	9-15
Very Stiff	16-30
Hard	over 30

SYMBOLS

DRILL HOLE	DN-K
OBSERVED WATER LEVEL	
Asphalt, Gravel Sub-base	
Brown Sandy WOOD	
Very Loose Brown Silty SAND	
Compact to Very Compact Brown/Gray Silty SAND, Gravelly w/ Wood Fibers	
Compact Brown/Black Gravelly SAND, Silty w/ Pcs. Asphalt	
Loose to Medium Compact Gray/Brown/Red GRAVEL	
BOULDER	
ROCK	

PREPARED BY: Stephen L. Borg 04/21/2022
 ALTERED BY: ON:



F.S. HEISER
 DIRECTOR
 GEOTECHNICAL ENGINEERING BUREAU

AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:	NY ROUTE 74 OVER EAGLE LAKE CHANNEL CULVERT REPLACEMENT SH 8230 SCHROON LAKE - TICONDEROGA, PART 2 TOWN OF TICONDEROGA COUNTY: ESSEX COUNTY REGION: 1	PIN 1110.02 UTILITY QUALITY LEVEL "C"	BRIDGES 1080770	CULVERTS C120086	ALL DIMENSIONS IN \pm UNLESS OTHERWISE NOTED	CONTRACT NUMBER D264836
BORING LOCATION PLAN					DRAWING NO. ST-7 SHEET NO. 30	
NY ROUTE 74 OVER EAGLE LAKE CHANNEL						

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LEGEND

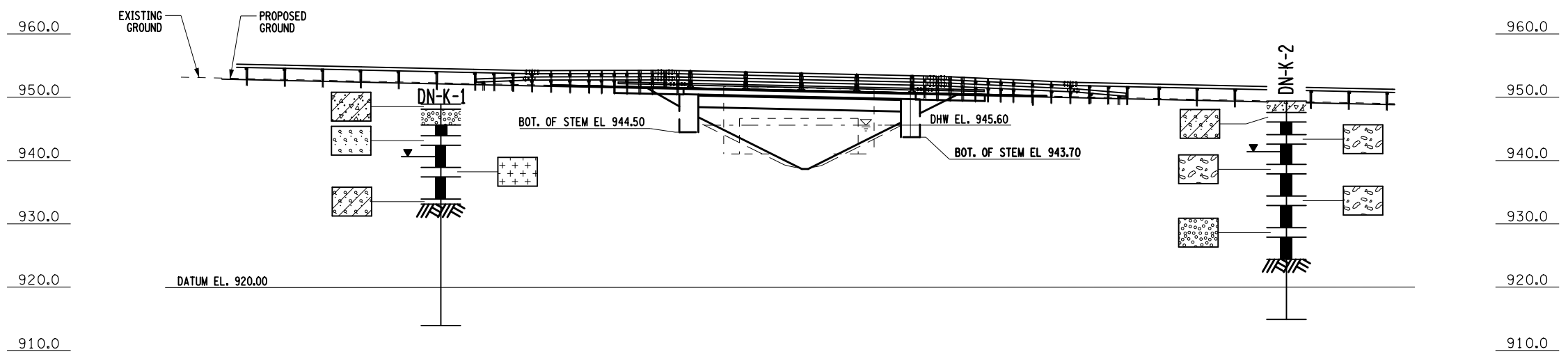
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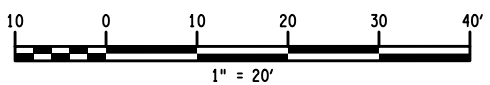
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SYMBOLS

DRILL HOLE	DN-K
OBSERVED WATER LEVEL	▼
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Brown Sandy WOOD	[Symbol]
Very Loose Brown Silty SAND	[Symbol]
Compact to Very Compact Brown/Gray Silty SAND, Gravelly w/ Wood Fibers	[Symbol]
Compact Brown/Black Gravelly SAND, Silty w/ Pcs. Asphalt	[Symbol]
Loose to Medium Compact Gray/Brown/Red GRAVEL	[Symbol]
BOULDER	[Symbol]
ROCK	[Symbol]



GENERAL SUBSURFACE PROFILE
ELEVATION A-A



PREPARED BY: Stephen L. Borg
04/21/2022

ALTERED BY:
ON:



F.S. HEISER
DIRECTOR
GEOTECHNICAL ENGINEERING
BUREAU

AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:	NY ROUTE 74 OVER EAGLE LAKE CHANNEL CULVERT REPLACEMENT SH 8230 SCHROON LAKE - TICONDEROGA, PART 2 TOWN OF TICONDEROGA COUNTY: ESSEX COUNTY REGION: 1	PIN 1110.02 UTILITY QUALITY LEVEL "C"	BRIDGES 1080770	CULVERTS C120086	ALL DIMENSIONS IN \pm UNLESS OTHERWISE NOTED GENERAL SUBSURFACE PROFILE NY ROUTE 74 OVER EAGLE LAKE CHANNEL	CONTRACT NUMBER D264836 DRAWING NO. ST-8 SHEET NO. 31
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IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

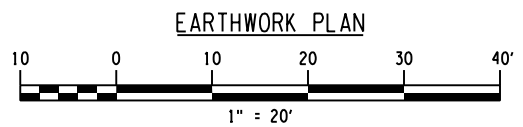
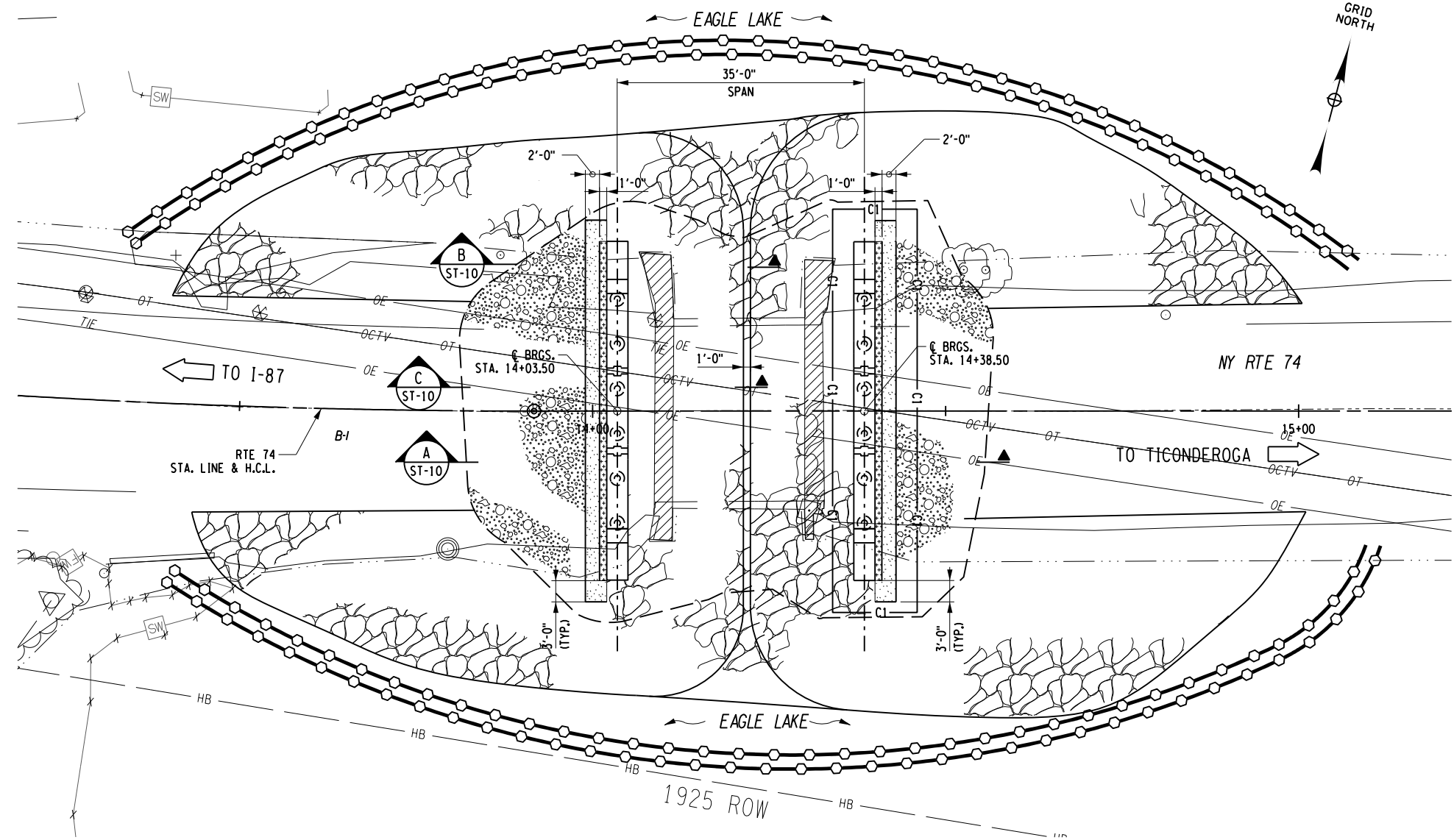


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CHECK
DESIGN S.L.J.
JOB MANAGER S.L.B.
DESIGN SUPERVISOR J.J.D. 4/21/22

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PROJECT MANAGER C. GEROUX
 CHECK A. BROWN
 DRAFTING K. DETRICK
 CHECK A. BROWN
 DESIGN L. SHORT
 JOB MANAGER C. GEROUX
 DESIGN SUPERVISOR J. PANGBURN



- LEGEND:**
- REMOVAL OF SUBSTRUCTURES, ITEM 202.19
 - HIGHWAY EMBANKMENT MATERIAL, ITEM 203.03
 - SELECT STRUCTURE FILL, ITEM 203.21
 - STONE FILLING (MEDIUM) ITEM 620.04
 - UNDERDRAIN FILTER, TYPE I, ITEM 605.0901
 - TURBIDITY CURTAIN, ITEM 209.1501
 - LIMITS OF STRUCTURE EXCAVATION, ITEM 206.01
 - COFFERDAM (TYPE 2), ITEM 553.020001

PREPARED BY: JEFFREY W. PANGBURN, P.E. ALTERED BY: ON:



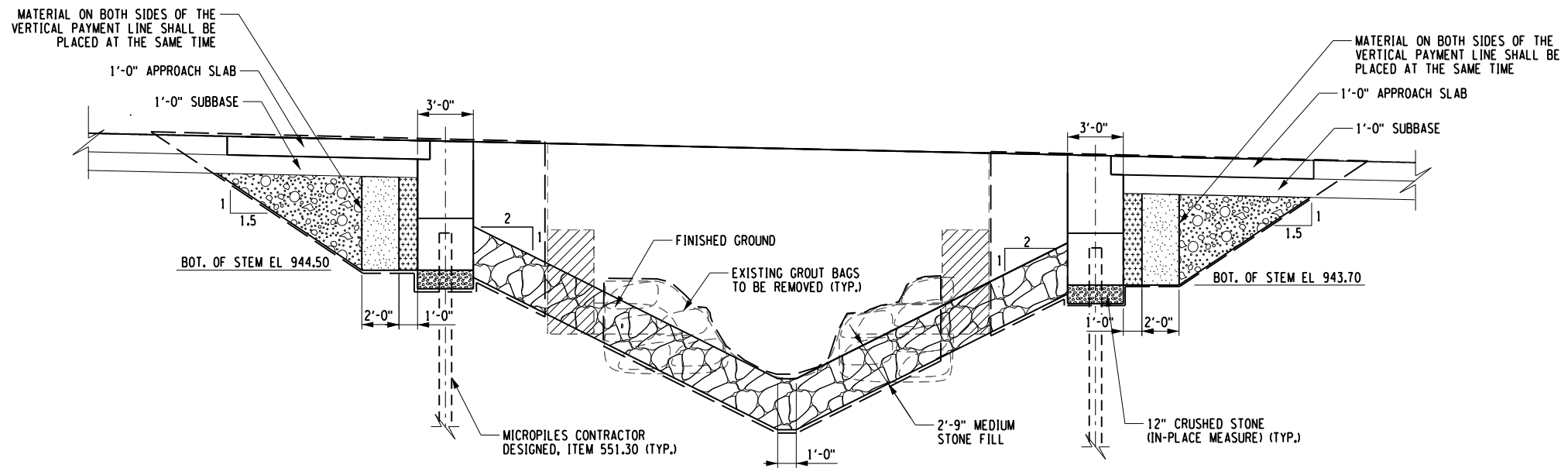
AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:	NY ROUTE 74 OVER EAGLE LAKE CHANNEL	PIN 1110.02	BRIDGES	CULVERTS	ALL DIMENSIONS IN FT UNLESS OTHERWISE NOTED	CONTRACT NUMBER
	CULVERT REPLACEMENT	UTILITY QUALITY LEVEL "C"	1080770	C120086		D264836
	SH 8230 SCHROON LAKE - TICONDEROGA, PART 2					
	TOWN OF TICONDEROGA	REGION: 1				
	COUNTY: ESSEX COUNTY				EARTHWORK PLAN	DRAWING NO. ST-9
						SHEET NO. 32

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

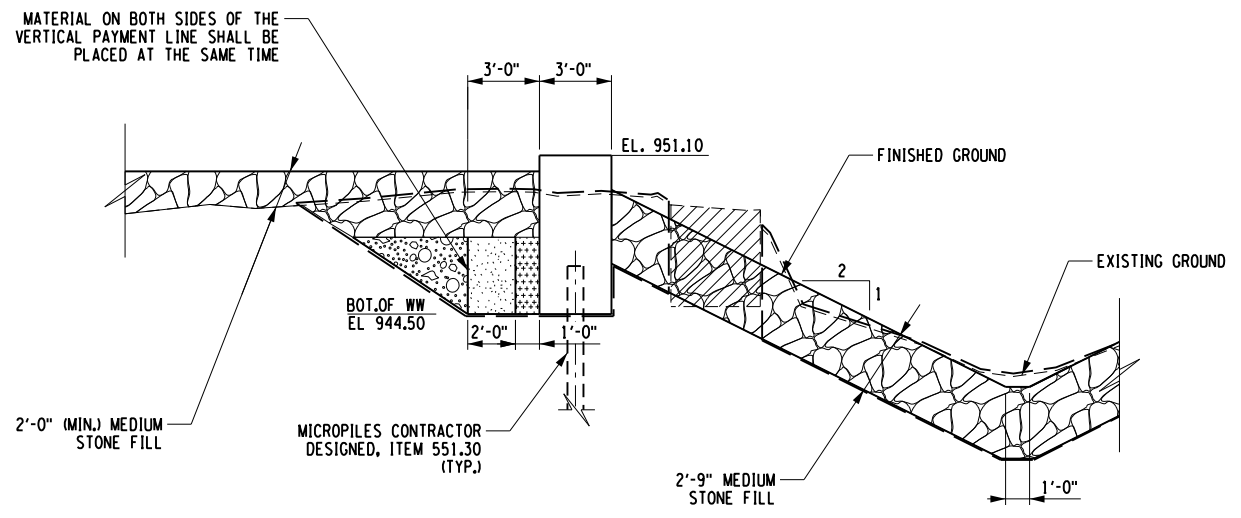


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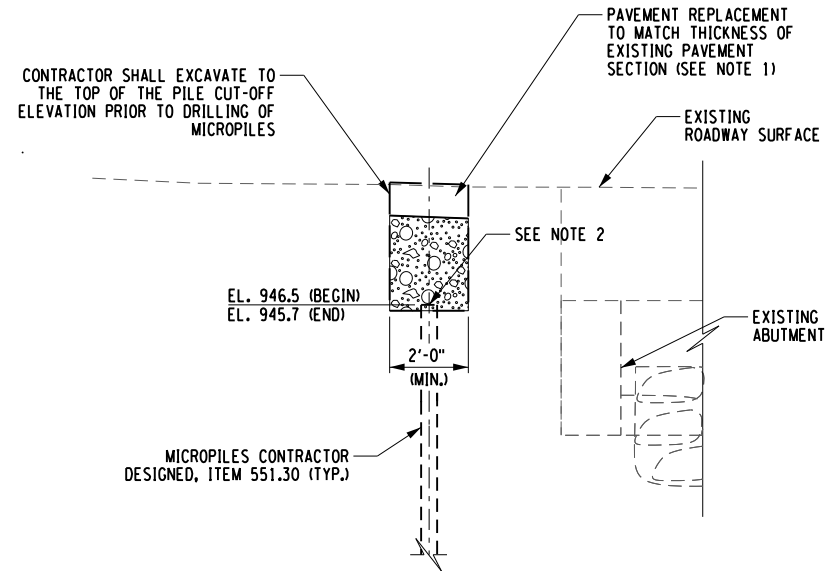
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 CHECK A. BROWN
 DRAFTING K. DETRICK
 CHECK A. BROWN
 DESIGN L. SHORT
 JOB MANAGER C. GEROUX
 DESIGN SUPERVISOR J. PANGBURN



A ABUTMENTS EARTHWORK SECTION
 ST-9 SCALE: 1/8"=1'-0"



B WINGWALL EARTHWORK SECTION
 ST-9 SCALE: 1/8"=1'-0"








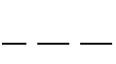


C PRE-EXCAVATION FOR PILE INSTALLATION
 ST-9 SCALE: 1/8"=1'-0"

NOTES:

1. CONTRACTOR SHALL RE-ESTABLISH ASPHALT WEARING SURFACE UTILIZING ITEM 405.01 - COLD MIX BITUMINOUS PAVEMENT IN 2" MAXIMUM LIFTS.
2. CONTRACTOR SHALL PROTECT MICROPILE REINFORCEMENT FROM DAMAGE DURING PLACEMENT OF HIGHWAY EMBANKMENT MATERIAL. INCLUDE COST IN MICROPILE ITEM.
3. DURING DEMOLITION OPERATIONS THE INTENT IS TO REMOVE THE EXISTING SUBSTRUCTURES IN THEIR ENTIRETY TO THE TOP OF EXISTING TIMBER CRIBBING. VARIOUS GROUT BAGS ARE LOCATED WITHIN THE CHANNEL. ALL EXISTING GROUT BAGS ARE TO BE REMOVED. COST FOR REMOVALS AND DISPOSAL SHALL BE PAID FOR UNDER THE RESPECTIVE REMOVAL ITEMS IN THE PLANS.

LEGEND:

-  REMOVAL OF SUBSTRUCTURES, ITEM 202.19
-  HIGHWAY EMBANKMENT MATERIAL, ITEM 203.03
-  SELECT STRUCTURE FILL, ITEM 203.21
-  STONE FILLING (MEDIUM) ITEM 620.04
-  UNDERDRAIN FILTER, TYPE 1, ITEM 605.0901
-  CRUSHED STONE (IN-PLACE MEASURE) ITEM 623.12
-  TURBIDITY CURTAIN, ITEM 209.1501
-  LIMITS OF STRUCTURE EXCAVATION, ITEM 206.01

PREPARED BY: JEFFREY W. PANGBURN, P.E. ALTERED BY: ON:



AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:	NY ROUTE 74 OVER EAGLE LAKE CHANNEL
	CULVERT REPLACEMENT
	SH 8230 SCHROON LAKE - TICONDEROGA, PART 2
	TOWN OF TICONDEROGA
	COUNTY: ESSEX COUNTY REGION: 1

PIN 1110.02	BRIDGES	CULVERTS	ALL DIMENSIONS IN FT UNLESS OTHERWISE NOTED	CONTRACT NUMBER
UTILITY QUALITY LEVEL "C"	1080770	C120086	EARTHWORK SECTIONS	D264836
				DRAWING NO. ST-10
			SHEET NO. 33	

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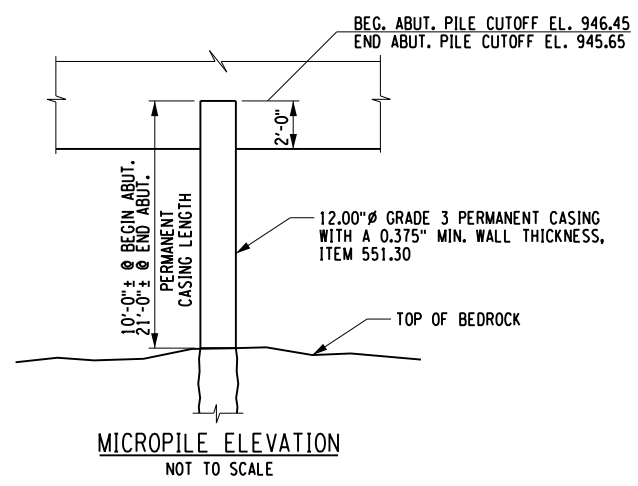
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 CHECK A. BROWN
 DRAFTING K. DETRICK
 CHECK A. BROWN
 DESIGN L. SHORT
 JOB MANAGER C. GEROUX
 DESIGN SUPERVISOR J. PANGBURN

MICROPILE NOTES:

1. THE MICROPILES AT THE ABUTMENTS WILL SUPPORT A MAXIMUM STRENGTH LIMIT STATE AXIAL LOAD OF 100 KIPS PER PILE. INSTALL THESE PILES TO ACHIEVE A NOMINAL RESISTANCE OF 182 KIPS PER PILE.
2. THE MAXIMUM SERVICE LIMIT STATE AXIAL LOAD FOR THE PILES AT THE ABUTMENTS IS 64 KIPS PER PILE.
3. DESIGN THE MICROPILES AT THE ABUTMENTS TO DEVELOP THEIR CAPACITY BELOW TOP OF COMPETENT ROCK ELEVATION.
4. MICROPILES WILL BE SOCKETED INTO BEDROCK. THE CONTRACTOR'S ENGINEER WILL DESIGN THE MICROPILES TO DEVELOP THE REQUIRED 182 KIPS NOMINAL AXIAL RESISTANCE FROM THE COMPETENT BEDROCK-GROUT BOND WITH SOCKET END BEARING DISCOUNTED. A MAXIMUM VALUE OF 158 PSI WILL BE USED FOR THE BEDROCK-GROUT BOND STRENGTH WHEN DETERMINING THE ROCK SOCKET LENGTH.
5. THE ASSUMED BEDROCK ELEVATIONS AT THE SPECIFIED DRILL-HOLE LOCATIONS ARE LISTED BELOW.

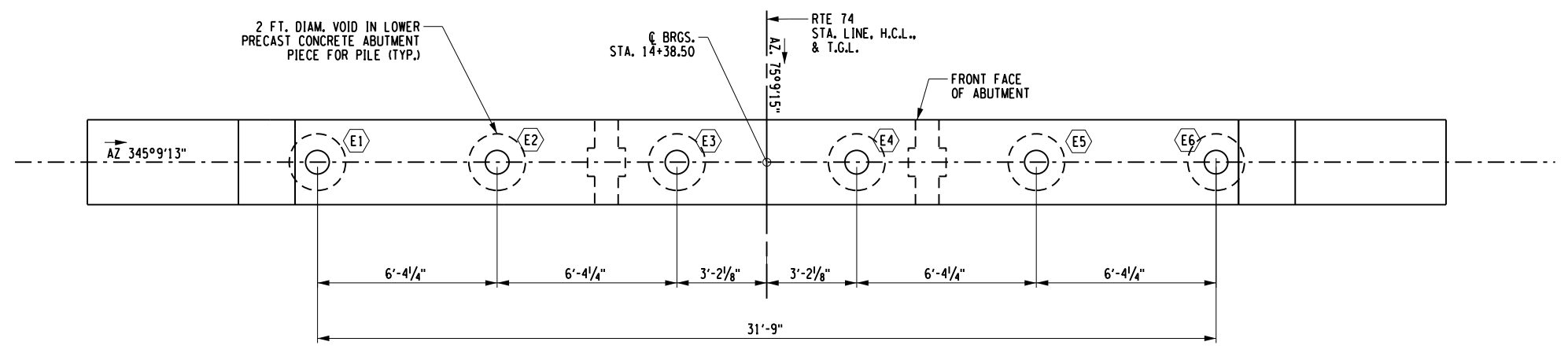
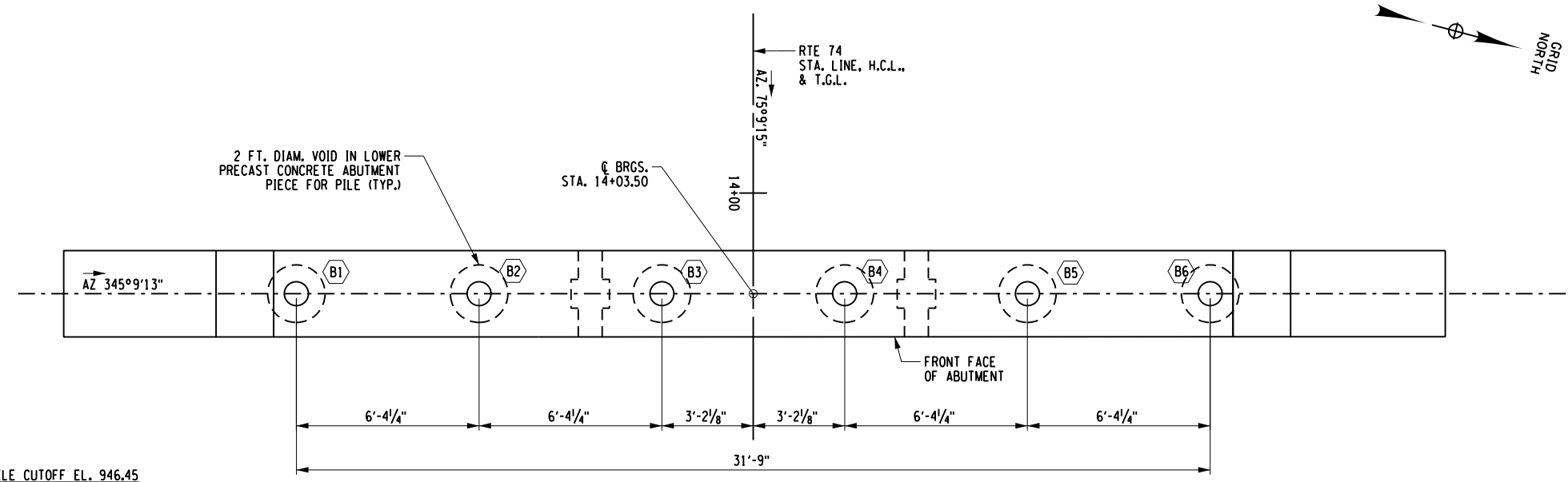
SUBSTRUCTURE	DRILL-HOLE	TOP OF BEDROCK ELEVATION (FEET)	TOP OF COMPETENT BEDROCK ELEVATION (FEET)
BEGIN ABUTMENT	DN-K-1	936.57	936.60
END ABUTMENT	DN-K-2	924.42	924.42

6. THE FINAL TOP OF COMPETENT BEDROCK AND TOP OF SOCKET ELEVATIONS AT EACH SUBSTRUCTURE SHALL BE DETERMINED BY AN ENGINEERING GEOLOGIST FROM THE GEOTECHNICAL ENGINEERING BUREAU DURING INSTALLATION OF THE MICROPILES.
7. AFTER COMPLETION OF THE MICROPILE INSTALLATION, THE ENGINEER WILL COMPLETE THE "ACTUAL MICROPILE LENGTH" TABLE FOR INCLUSION IN THE ASBUILT PLANS.
8. THE WATER LEVELS NOTED ON THE BORING LOGS AND ON THE SUBSURFACE PROFILE DRAWING INCLUDED IN THE CONTRACT PLANS FOR THIS STRUCTURE MAY NOT BE INDICATIVE OF ACTUAL WATER CONDITIONS AT THE TIME OF CONSTRUCTION.



ACTUAL PILE LENGTH	
PILE NO.	LENGTH BELOW CUT-OFF
B1	
B2	
B3	
B4	
B5	
B6	

ACTUAL PILE LENGTH	
PILE NO.	LENGTH BELOW CUT-OFF
E1	
E2	
E3	
E4	
E5	
E6	



PREPARED BY: JEFFREY W. PANGBURN, P.E. ALTERED BY: ON:



AS-BUILT REVISIONS	DESCRIPTION OF ALTERATIONS:	NY ROUTE 74 OVER EAGLE LAKE CHANNEL	PIN 1110.02	BRIDGES	CULVERTS	ALL DIMENSIONS IN FT UNLESS OTHERWISE NOTED	CONTRACT NUMBER
		CULVERT REPLACEMENT	UTILITY QUALITY LEVEL "C"	1080770	C120086	PILE LAYOUT AND DETAILS	D264836
		SH 8230 SCHROON LAKE - TICONDEROGA, PART 2					DRAWING NO. ST-11
		TOWN OF TICONDEROGA					SHEET NO. 34

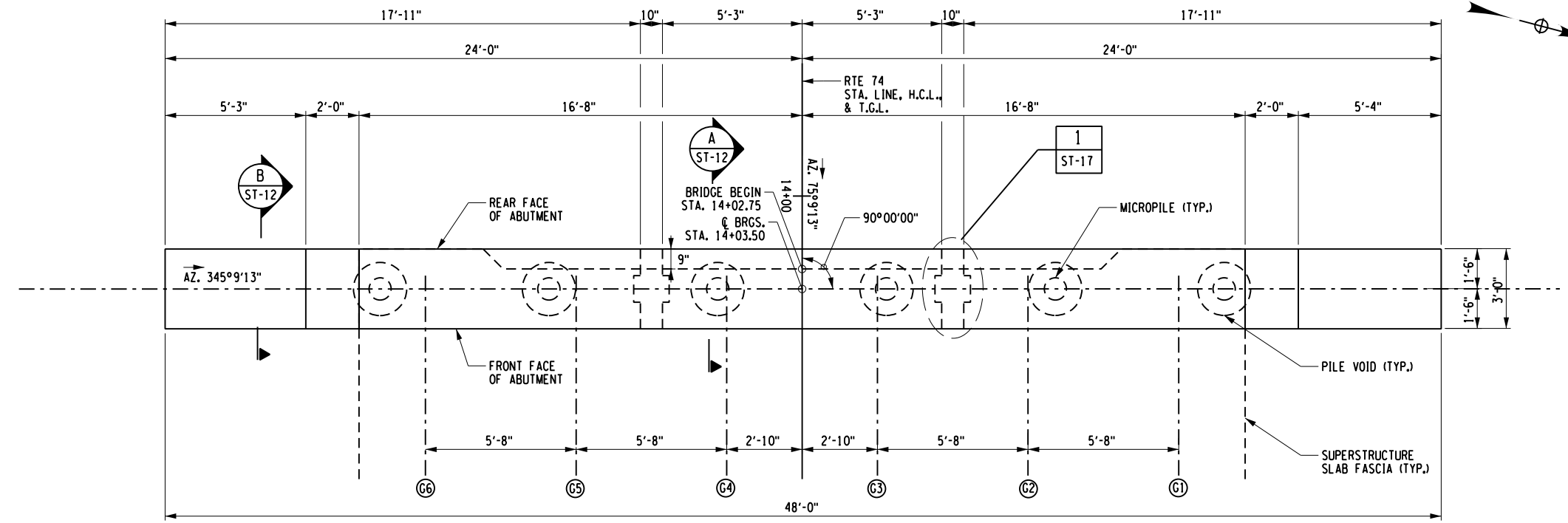
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LEGEND:
 ⊕ MICROPILES (CONTRACTOR DESIGNED, ITEM 551.30)
 ① INDICATES PILE NUMBER

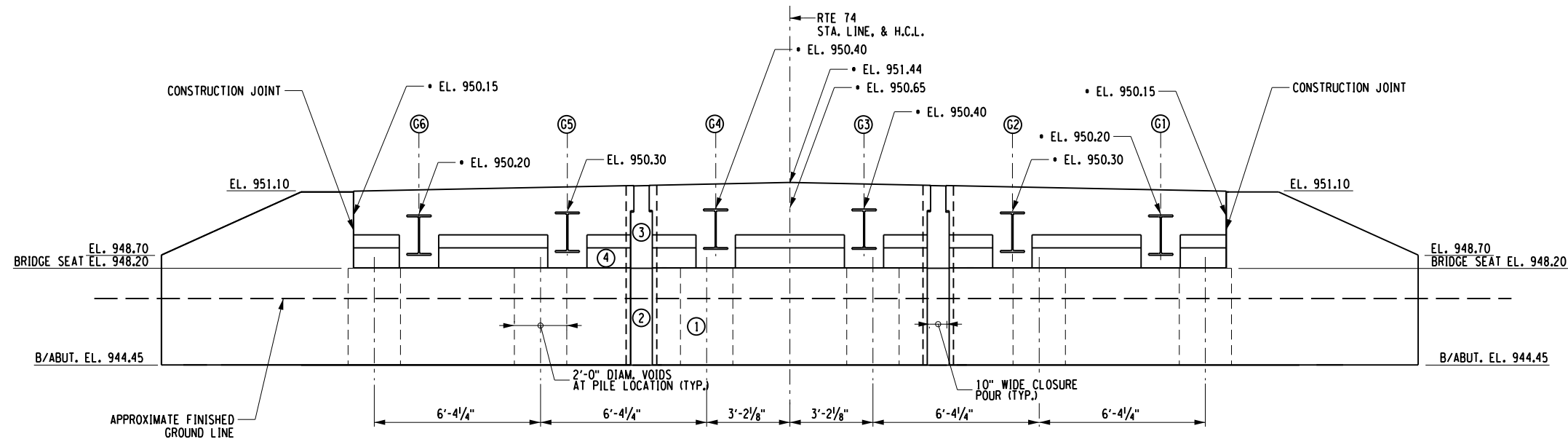


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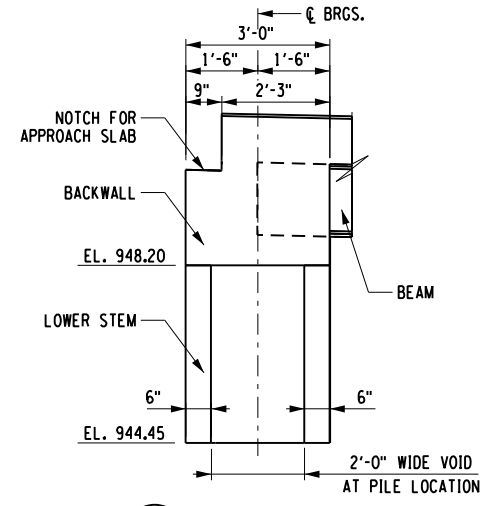
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 DESIGN: L. SHORT
 CHECK: A. BROWN
 DRAFTING: K. DETRICK
 CHECK: A. BROWN
 PROJECT MANAGER: C. GEROUX



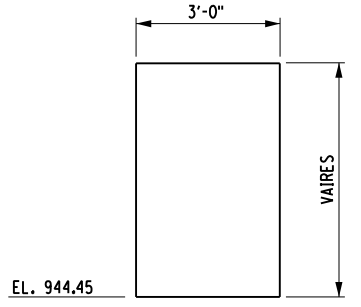
BEGIN ABUTMENT PLAN
 SCALE: 3/16"=1'-0"



BEGIN ABUTMENT ELEVATION
 SCALE: 3/16"=1'-0"



A ABUTMENT SECTION
 SCALE: 1/4"=1'-0"



B WINGWALL SECTION
 SCALE: 1/4"=1'-0"

- NOTES:**
1. REAR FACE OF ABUTMENT NOT SHOWN IN THE ELEVATION VIEW FOR CLARITY.
 2. PILES NOT SHOWN FOR CLARITY.
 3. TOP OF WINGWALL TO FOLLOW GRADE OF ROADWAY.
 4. THE LOWER PRECAST CONCRETE ABUTMENT STEMS AND WINGWALLS SHALL BE PAID FOR UNDER ITEM 555.90000009. THE COST FOR THE UPPER PRECAST CONCRETE ABUTMENT STEM/BACKWALL SHALL BE INCLUDED IN ITEM 557.65010103 - PREFABRICATED MODULAR DECK BEAMS.
 5. IT IS ASSUMED THE CLOSURE POURS FOR THE BACKWALL AND DECK WILL OCCUR IN A CONTINUOUS POUR. THE REPORTED VOLUME INCLUDES THE BACKWALL AND HALF OF THE DECK.

- LEGEND:**
- ① INDICATES POUR NUMBER
 - ⓐ INDICATES GIRDER NUMBER

• THESE ELEVATIONS ARE TAKEN AT Q OF BEARINGS

CLOSURE POURS	
PLACEMENT	CF
① PILE VOIDS	58
② STEM CLOSURES	25
③ BACKWALL/DECK CLOSURES	39
④ FACE CLOSURE	27

PREPARED BY: JEFFREY W. PANGBURN, P.E. ALTERED BY: ON:



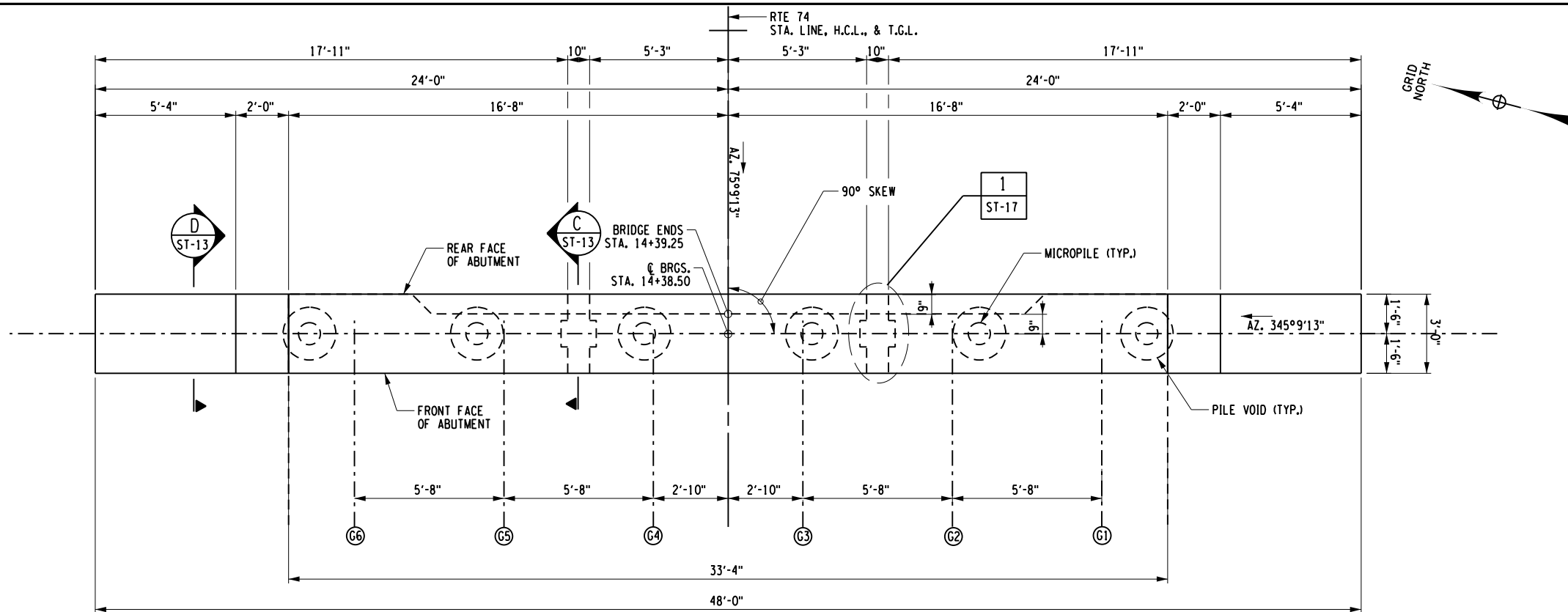
AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:	NY ROUTE 74 OVER EAGLE LAKE CHANNEL CULVERT REPLACEMENT SH 8230 SCHROON LAKE - TICONDEROGA, PART 2 TOWN OF TICONDEROGA COUNTY: ESSEX COUNTY REGION: 1	PIN 1110.02 UTILITY QUALITY LEVEL "C"	BRIDGES 1080770	CULVERTS C120086	ALL DIMENSIONS IN FT UNLESS OTHERWISE NOTED	CONTRACT NUMBER D264836
BEGIN ABUTMENT PLAN AND ELEVATION					DRAWING NO. ST-12 SHEET NO. 35	

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

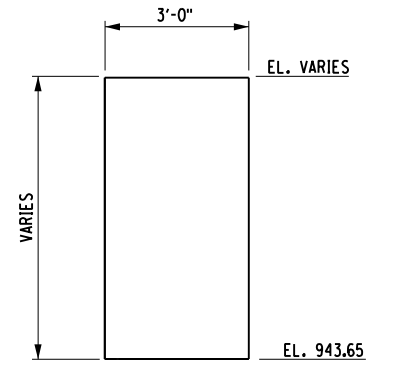


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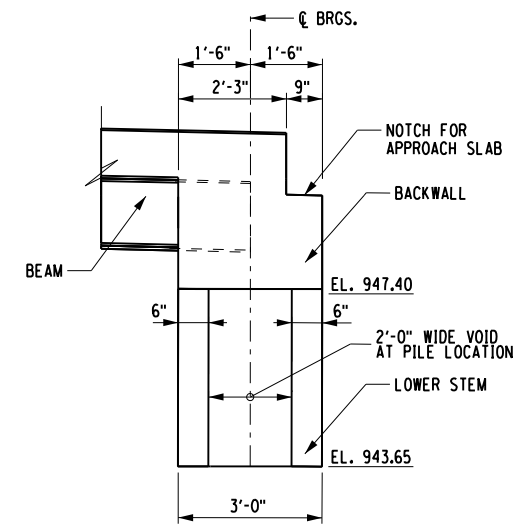
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 JOB MANAGER J. PANGBURN



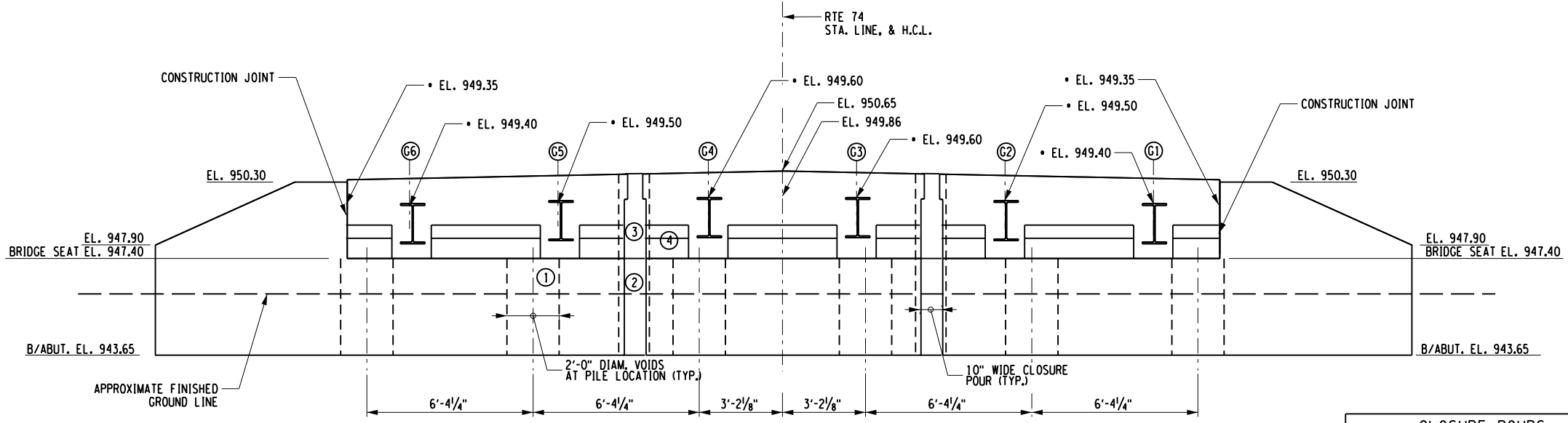
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D WINGWALL SECTION
 ST-13
 SCALE: 1/4"=1'-0"



C END ABUTMENT SECTION
 ST-13
 SCALE: 1/4"=1'-0"



END ABUTMENT ELEVATION
 SCALE: 3/16"=1'-0"

CLOSURE POURS	
PLACEMENT	CF
① PILE VOIDS	58
② STEM CLOSURES	25
③ BACKWALL/DECK CLOSURES	39
④ FACE CLOSURE	27

- NOTES:
- REAR FACE OF ABUTMENT NOT SHOWN IN THE ELEVATION VIEW FOR CLARITY.
 - PILES NOT SHOWN FOR CLARITY.
 - TOP OF WINGWALL TO FOLLOW GRADE OF ROADWAY.
 - THE LOWER PRECAST CONCRETE ABUTMENT STEMS AND WINGWALLS SHALL BE PAID FOR UNDER ITEM 555.9000009. THE COST FOR THE UPPER PRECAST CONCRETE ABUTMENT STEM/BACKWALL SHALL BE INCLUDED IN ITEM 557.65010103 - PREFABRICATED MODULAR DECK BEAMS.
 - IT IS ASSUMED THE CLOSURE POURS FOR THE BACKWALL AND DECK WILL OCCUR IN A CONTINUOUS POUR, THE REPORTED VOLUME INCLUDES THE BACKWALL AND HALF OF THE DECK.

PREPARED BY: JEFFREY W. PANGBURN, P.E.
 ALTERED BY: ON:



LEGEND:
 ① INDICATES POUR NUMBER
 ⑥ INDICATES GIRDER NUMBER
 • THESE ELEVATIONS ARE TAKEN AT C. OF BEARINGS

AS-BUILT REVISIONS	DESCRIPTION OF ALTERATIONS:
	NY ROUTE 74 OVER EAGLE LAKE CHANNEL
	CULVERT REPLACEMENT
	SH 8230 SCHROON LAKE - TICONDEROGA, PART 2
	TOWN OF TICONDEROGA
	COUNTY: ESSEX COUNTY REGION: 1

PIN 1110.02	BRIDGES	CULVERTS	ALL DIMENSIONS IN FT UNLESS OTHERWISE NOTED	CONTRACT NUMBER
UTILITY QUALITY LEVEL "C"	1080770	C120086		D264836

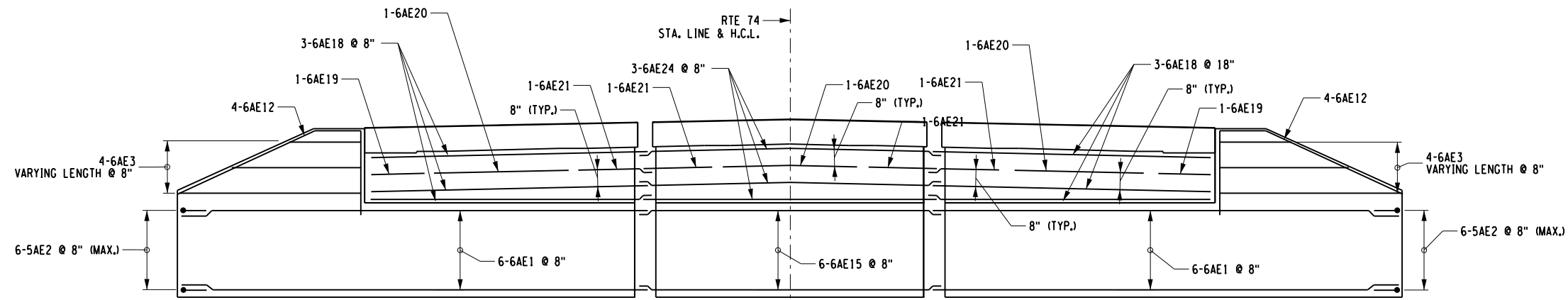
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		SHEET NO. 36

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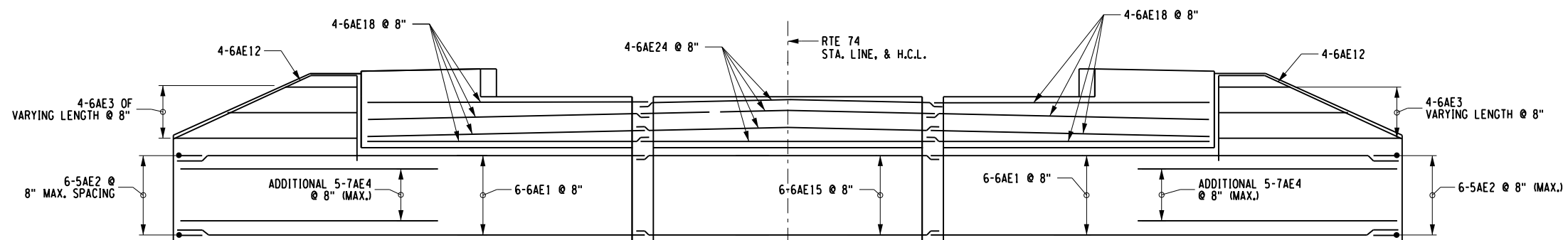


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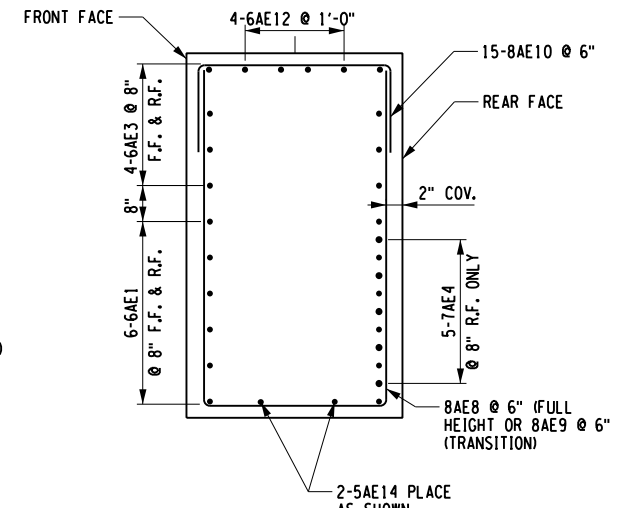
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 CHECK A. BROWN
 DRAFTING K. DETRICK
 CHECK A. BROWN
 DESIGN L. SHORT
 JOB MANAGER C. GEROUX
 DESIGN SUPERVISOR J. PANGBURN



BEGIN/END ABUTMENT FRONT FACE ELEVATION
 (ONLY HORIZONTAL BARS SHOWN)
 (NOT TO SCALE)



BEGIN/END ABUTMENT REAR FACE ELEVATION
 (ONLY HORIZONTAL BARS SHOWN)
 SCALE: 3/16"=1'-0"



B CANTILEVER WINGWALL SECTION
 SCALE: 3/8"=1'-0"

NOTES:
 (E) DENOTES EPOXY COATED BARS.
 COVER FOR STEEL REINFORCEMENT SHALL BE 1/2" UNLESS OTHERWISE NOTED.
 REINFORCEMENT FOR BEGIN AND END ABUTMENTS IS IDENTICAL.

PREPARED BY: JEFFREY W. PANGBURN, P.E. ALTERED BY: ON:

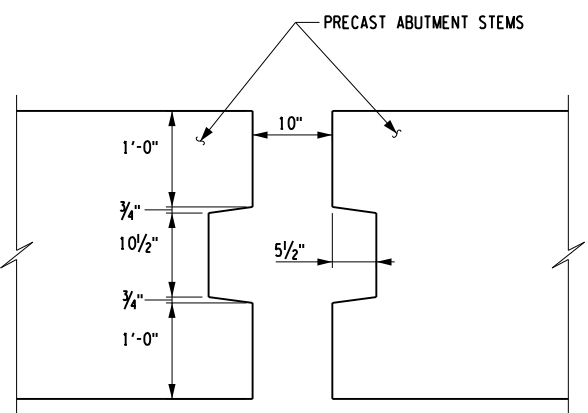


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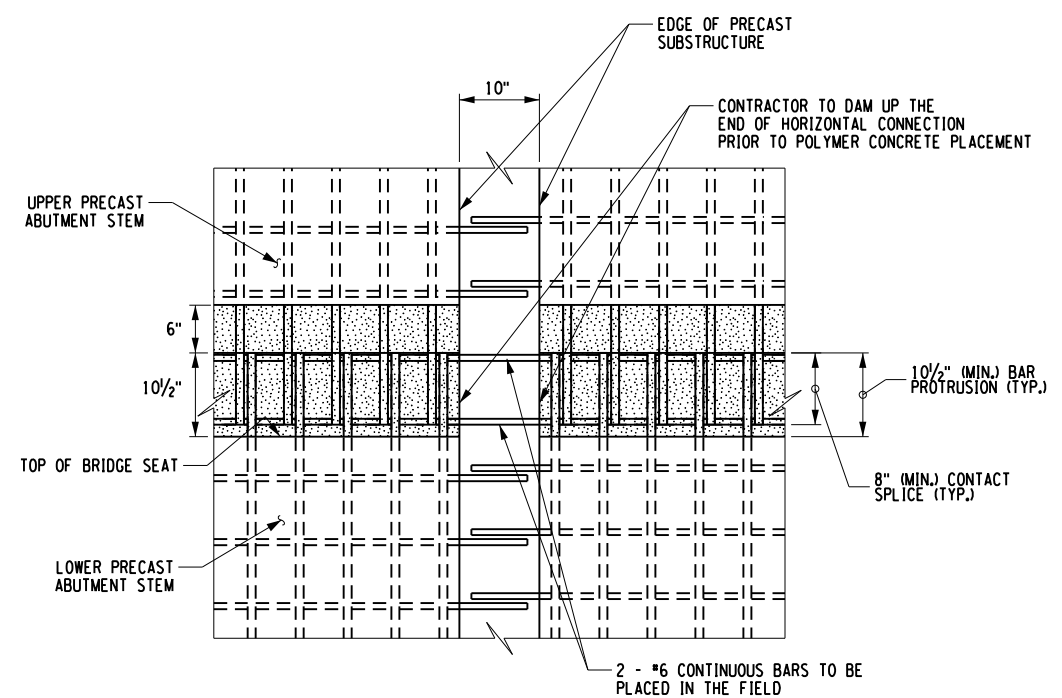


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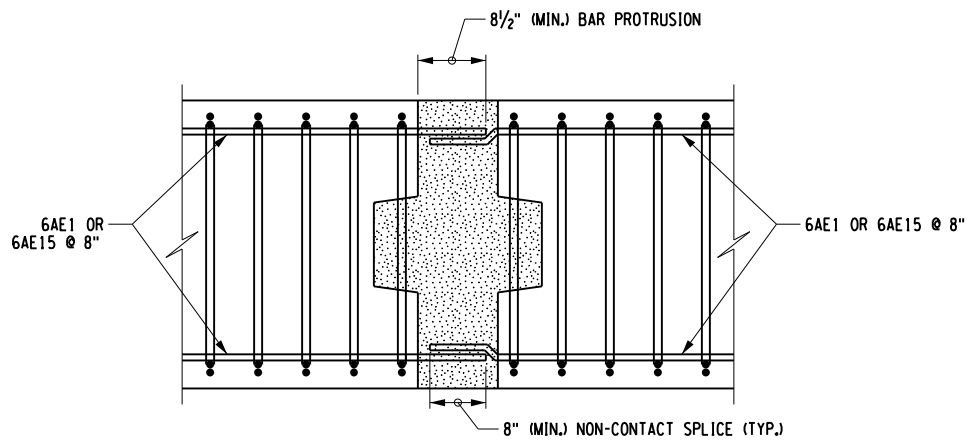
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 CHECK A. BROWN
 PROJECT MANAGER C. GEROUX



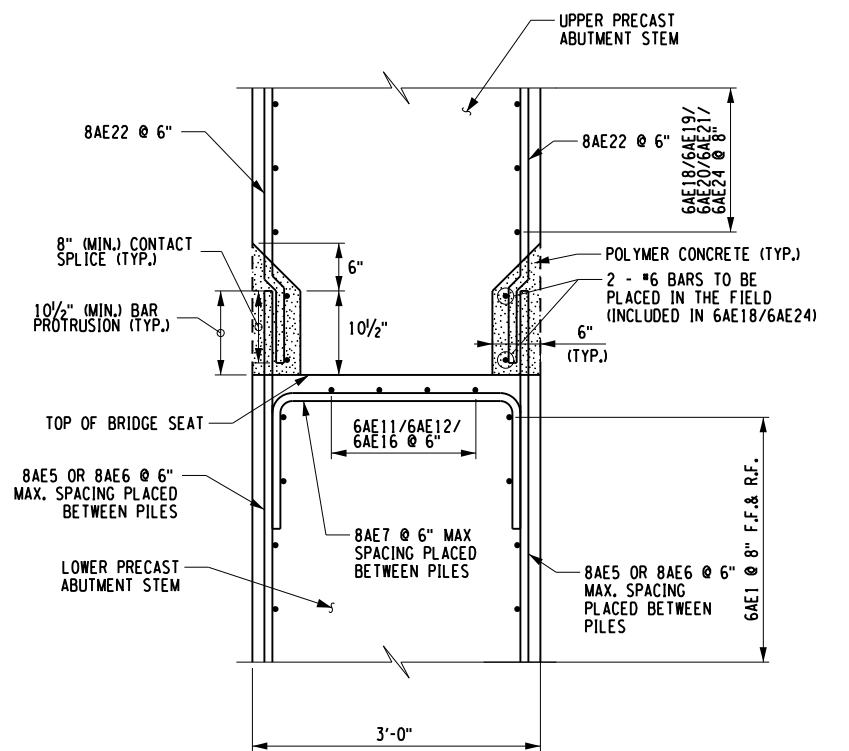
VERTICAL - ABUTMENT KEYWAY DETAIL
 SCALE: 1/2"=1'-0"



HORIZONTAL - UPPER & LOWER ABUTMENT CONNECTION ELEVATION
 SCALE: 1/2"=1'-0"



VERTICAL - ABUTMENT CLOSURE POUR DETAIL
 NOT TO SCALE



HORIZONTAL - UPPER & LOWER ABUTMENT CONNECTION DETAIL
 SCALE: 1/2"=1'-0"

NOTES:
 1. THE COST OF THE POLYMER CONCRETE SHALL BE INCLUDED IN THE UNIT BID PRICE FOR ITEM 557.24010101.

POLYMER CONCRETE PERFORMANCE CRITERIA:
 CURE TIMES:
 1. THE POLYMER CONCRETE MIX SHALL ACHIEVE 6,000 PSI STRENGTH IN 90 MINUTES.
 2. FINAL STRENGTH OF 10,000 PSI SHALL BE ACHIEVED IN 4 TO 6 HOURS.

LEGEND:
 - POLYMER CONCRETE, ITEM 557.24010101

PREPARED BY: JEFFREY W. PANGBURN, P.E. ALTERED BY: ON:



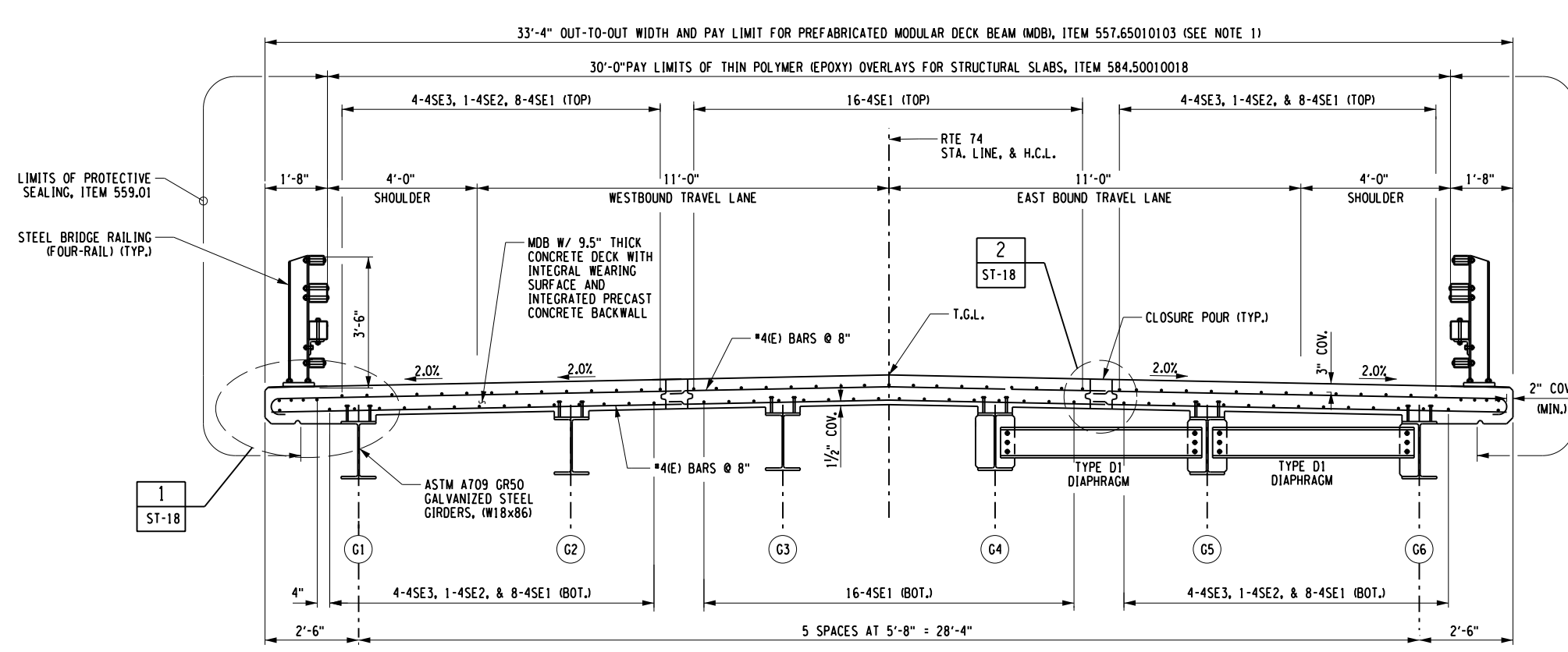
AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:	NY ROUTE 74 OVER EAGLE LAKE CHANNEL	PIN 1110.02	BRIDGES	CULVERTS	ALL DIMENSIONS IN FT UNLESS OTHERWISE NOTED	CONTRACT NUMBER D264836
	CULVERT REPLACEMENT	UTILITY QUALITY LEVEL "C"	1080770	C120086		
	SH 8230 SCHROON LAKE - TICONDEROGA, PART 2					
	TOWN OF TICONDEROGA					
	COUNTY: ESSEX COUNTY	REGION: 1			ABUTMENT DETAILS - 2	DRAWING NO. ST-17
						SHEET NO. 40

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY, IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

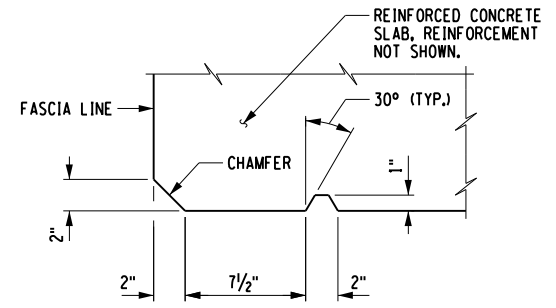


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 USER = KDetrick
 PLOT = NYS DOT CME PDF.plt:cfg

PROJECT MANAGER C. GEROUX
 CHECK A. BROWN
 DRAFTING K. DETRICK
 CHECK A. BROWN
 DESIGN L. SHORT
 JOB MANAGER C. GEROUX
 DESIGN SUPERVISOR J. PANGBURN

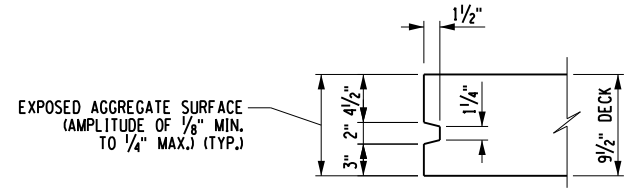


TRANSVERSE SECTION
 SCALE: 1/4"=1'-0"

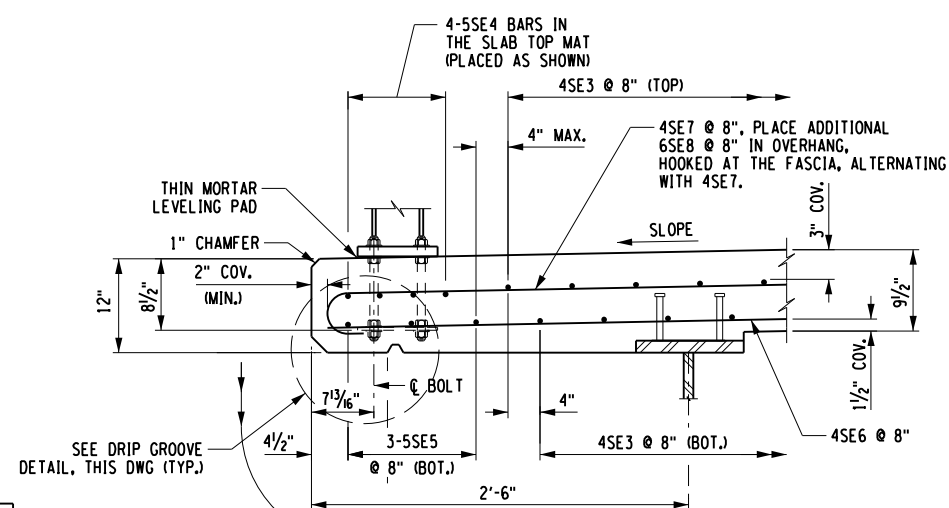


DRIP GROOVE DETAIL
 (NOT TO SCALE)

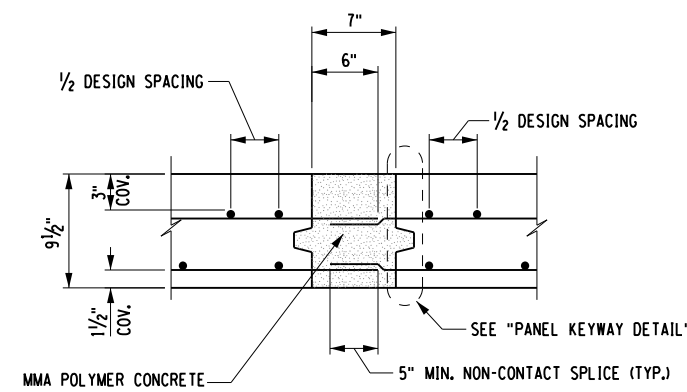
NOTE:
 DRIP GROOVE STOPS 3'-0" FROM FACES OF ABUTMENTS OR PIERS WITH A 90° TURN TOWARD FASCIA THAT INTERSECTS THE CHAMFER.



PANEL KEYWAY DETAIL
 (NOT TO SCALE)



1 FASCIA DETAIL
 ST-18 (STEEL BRIDGE RAIL CURBLESS)
 (NOT TO SCALE)



2 CLOSURE POUR DETAIL
 ST-18
 (NOT TO SCALE)

NOTE:
 1. ITEMS 557.65010103 COST SHALL INCLUDE THE SUPERSTRUCTURE ELEMENTS DESCRIBED IN THE SPECIAL SPECIFICATIONS EXCEPT THAT CLOSURE POURS WILL BE PAID UNDER A SEPRATE ITEM.

PREPARED BY: JEFFREY W. PANGBURN, P.E. ALTERED BY: ON:



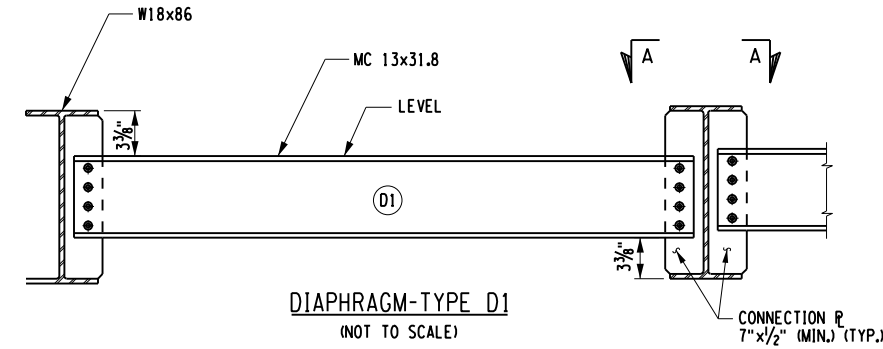
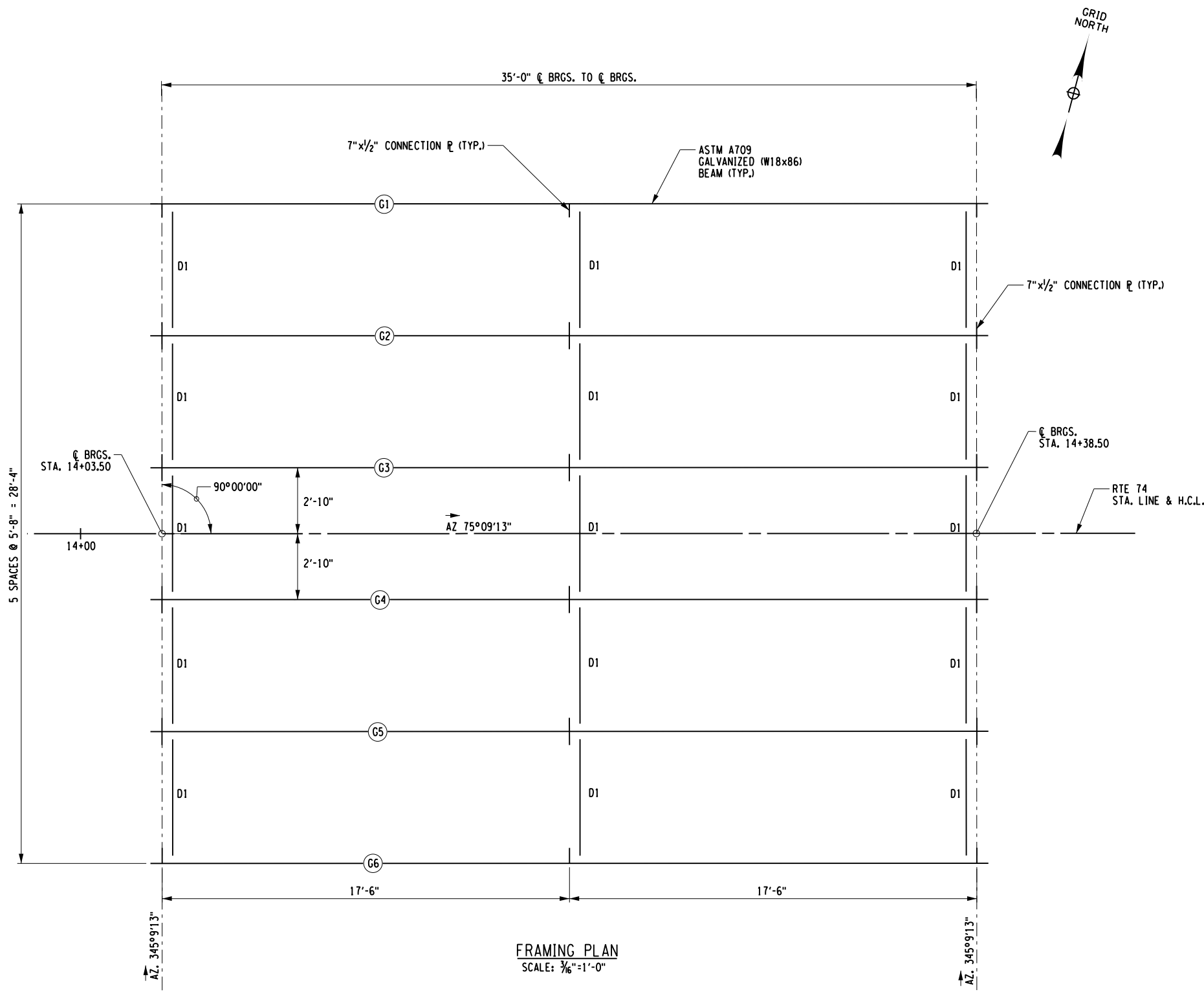
AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:	NY ROUTE 74 OVER EAGLE LAKE CHANNEL	PIN 1110.02	BRIDGES	CULVERTS	ALL DIMENSIONS IN FT UNLESS OTHERWISE NOTED	CONTRACT NUMBER
	CULVERT REPLACEMENT	UTILITY QUALITY LEVEL "C"	1080770	C120086		D264836
	SH 8230 SCHROON LAKE - TICONDEROGA, PART 2	TOWN OF TICONDEROGA	REGION: 1	TRANSVERSE SECTION		DRAWING NO. ST-18
	COUNTY: ESSEX COUNTY					SHEET NO. 41

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DESIGN SUPERVISOR J. PANGBURN
 JOB MANAGER C. GEROUX
 DESIGN L. SHORT
 CHECK A. BROWN
 DRAFTING K. DETRICK
 CHECK A. BROWN
 PROJECT MANAGER C. GEROUX



- NOTES:**
1. CONNECTIONS SHALL BE MADE ACCORDING TO THE NEW YORK STATE STEEL CONSTRUCTION MANUAL.
 2. UNLESS OTHERWISE INDICATED, BOLTED CONNECTIONS SHALL BE MADE WITH 3/8" DIA. A325 HIGH-STRENGTH BOLTS.
 3. THE CONTRACTOR MAY PLACE DIAPHRAGMS ON EITHER SIDE OF THE BEARING STIFFENERS OR CONNECTION PLATES AS NECESSARY TO CORRECT ALIGNMENT PROVIDED THERE WILL BE NO INTERFERENCE WITH OTHER STRUCTURAL DETAILS.
 4. ALL STRUCTURAL STEEL INCLUDING BUT NOT LIMITED TO STEEL GIRDERS, CONNECTION PLATES, AND DIAPHRAGMS SHALL BE PROVIDED FOR UNDER ITEM 557.65010103.
 5. ALL STRUCTURAL STEEL SHALL BE HOT DIPPED GALVANIZED PER § 719-01

FRAMING PLAN
 SCALE: 3/16"=1'-0"

PREPARED BY: JEFFREY W. PANGBURN, P.E. ALTERED BY: ON:



AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:	NY ROUTE 74 OVER EAGLE LAKE CHANNEL CULVERT REPLACEMENT SH 8230 SCHROON LAKE - TICONDEROGA, PART 2 TOWN OF TICONDEROGA COUNTY: ESSEX COUNTY REGION: 1
---	---

PIN 1110.02 UTILITY QUALITY LEVEL "C"	BRIDGES 1080770	CULVERTS C120086
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ALL DIMENSIONS IN FT UNLESS OTHERWISE NOTED		CONTRACT NUMBER D264836
FRAMING PLAN		DRAWING NO. ST-19
		SHEET NO. 42

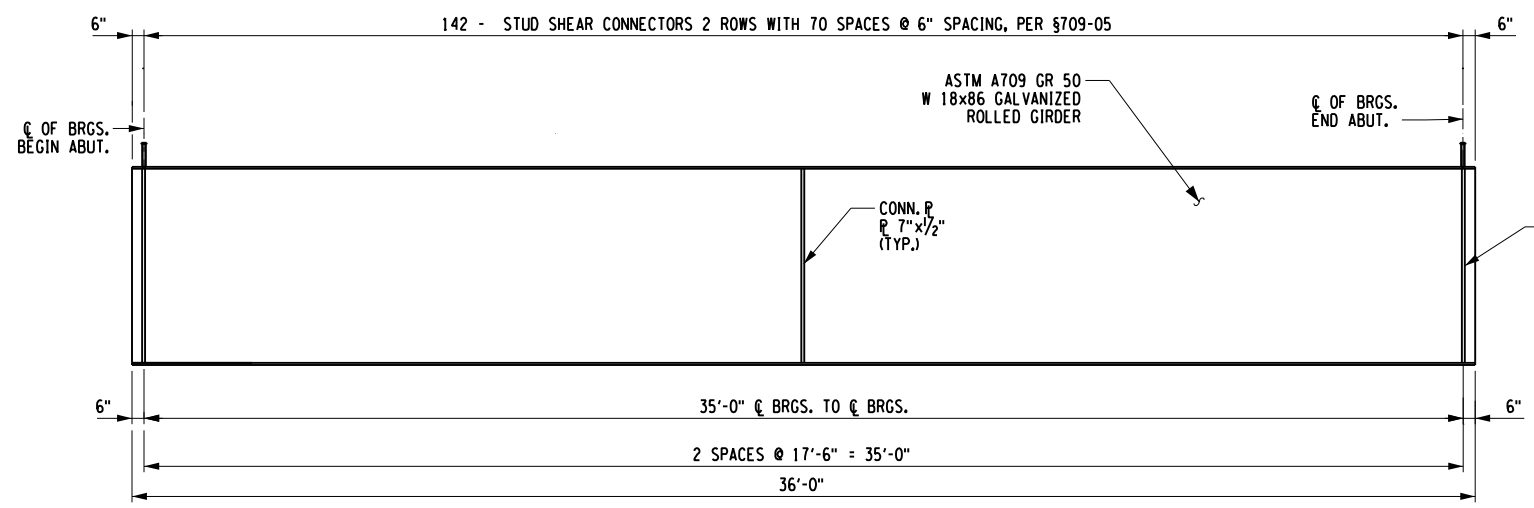
LEGEND:
 (GX) - INDICATES GIRDER NO.

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY, IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

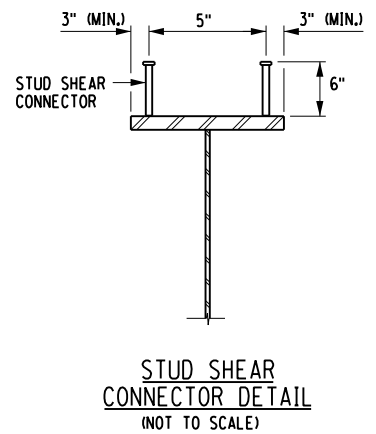


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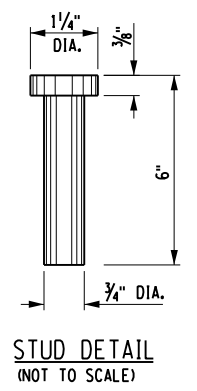
DESIGN SUPERVISOR J. PANGBURN
 DESIGN L. SHORT
 CHECK A. BROWN
 DRAFTING K. DETRICK
 CHECK A. BROWN
 PROJECT MANAGER C. GEROUX



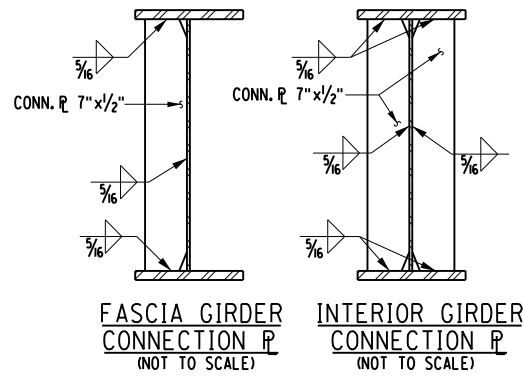
GIRDER ELEVATION
NOT TO SCALE



STUD SHEAR CONNECTOR DETAIL
(NOT TO SCALE)



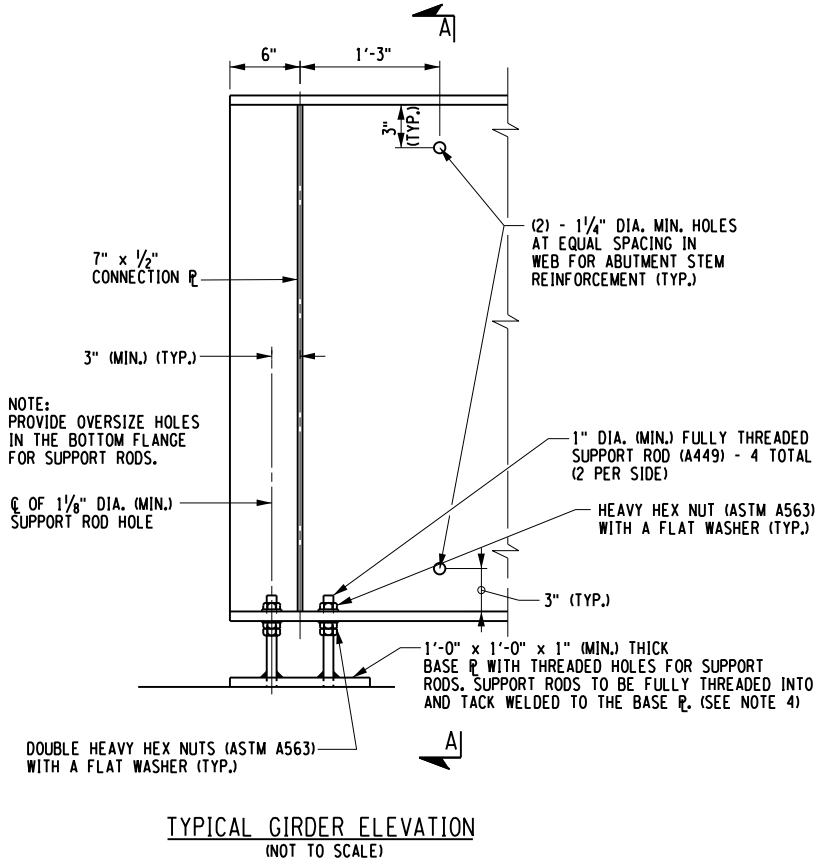
STUD DETAIL
(NOT TO SCALE)



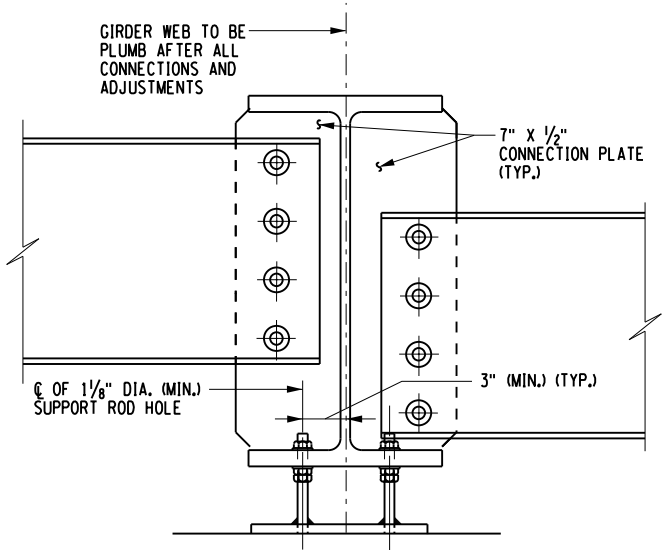
FASCIA GIRDER CONNECTION PLATE
(NOT TO SCALE)

INTERIOR GIRDER CONNECTION PLATE
(NOT TO SCALE)

- NOTES:
1. THE ENDS OF ALL GIRDERS AND THE CONNECTION PLATES SHALL BE VERTICAL. ALL CONNECTION PLATES AND INTERMEDIATE STIFFENERS MAY BE PERPENDICULAR TO THE TOP FLANGES.
 2. DETAILS ON THE DRAWINGS LABELED AS "NOT TO SCALE" ARE INTENTIONALLY DRAWN NOT TO SCALE FOR VISUAL CLARITY. ALL OTHER DETAILS, FOR WHICH NO SCALE IS SHOWN, ARE DRAWN PROPORTIONAL AND ARE FULLY DIMENSIONED.
 3. THE COST OF ALL STRUCTURAL STEEL SHALL BE INCLUDED IN ITEM 557.65010103.
 - C.P.G.W. = COMPLETE PENETRATION GROOVE WELD
 - M.B. = MILL TO BEAR THEN FILLET WELDED
 4. FABRICATOR MAY SUBMIT ALTERNATIVE SUPPORT METHODS.



TYPICAL GIRDER ELEVATION
(NOT TO SCALE)



SECTION A-A END DIAPHRAGM (D1)
(NOT TO SCALE)

PREPARED BY: JEFFREY W. PANGBURN, P.E. ALTERED BY: ON:



AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:	NY ROUTE 74 OVER EAGLE LAKE CHANNEL	PIN 1110.02	BRIDGES	CULVERTS	ALL DIMENSIONS IN FT UNLESS OTHERWISE NOTED	CONTRACT NUMBER
	CULVERT REPLACEMENT	UTILITY QUALITY LEVEL "C"	1080770	C120086		D264836
	SH 8230 SCHROON LAKE - TICONDEROGA, PART 2				GIRDER ELEVATION AND DETAILS	DRAWING NO. ST-20
	TOWN OF TICONDEROGA					SHEET NO. 43
	COUNTY: ESSEX COUNTY	REGION: 1				

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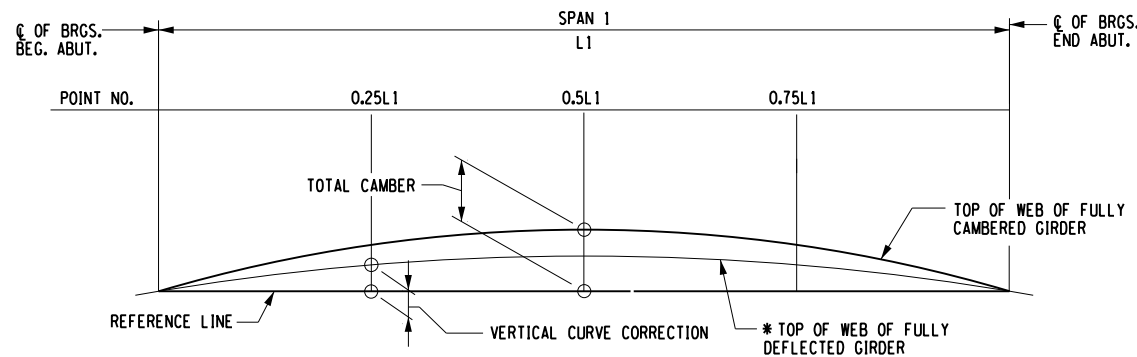
DESIGN SUPERVISOR J. PANGBURN
 DESIGN L. SHORT
 CHECK A. BROWN
 DRAFTING K. DETRICK
 CHECK A. BROWN
 PROJECT MANAGER C. GEROUX

MOMENT & SHEAR TABLE		C OF BRGS. BEG. ABUT.					C OF BRGS. END. ABUT.				
		0.25 L	0.50 L	0.75 L							
GIRDERS 1 & 6	D.L.	MOMENT	0.0	91.3	122.1	91.3	0.0				
		SHEAR	13.9	7.0	0.0	-7.0	-13.9				
	S.D.L	MOMENT	0.0	17.9	23.8	17.9	0.0				
		SHEAR	2.7	1.4	0.0	1.4	2.7				
	HL-93(+)	MOMENT	0.0	258.5	301.5	258.5	0.0				
		SHEAR	43.6	28.4	15.2	6.1	0.0				
HL-93(-)	MOMENT	0.0	0.0	0.0	0.0	0.0					
	SHEAR	0.0	-6.1	-15.2	-28.4	-43.6					
GIRDERS 2, 3, 4, & 5	D.L.	MOMENT	0.0	91.0	121.9	91.0	0.0				
		SHEAR	13.8	7.0	0.0	-7.0	-13.8				
	S.D.L	MOMENT	0.0	17.8	23.8	17.8	0.0				
		SHEAR	2.7	1.4	0.0	-1.4	0.0				
	HL-93(+)	MOMENT	0.0	31.3	18.3	7.3	0.0				
		SHEAR	52.6	34.3	18.3	7.3	0.0				
HL-93(-)	MOMENT	0.0	0.0	0.0	0.0	0.0					
	SHEAR	0.0	-7.3	-18.3	-34.3	-52.6					

MOMENTS AND SHEARS ARE UNFACTORED
 MOMENTS ARE EXPRESSED AS KIP-FEET
 SHEARS ARE EXPRESSED AS KIPS

CAMBER TABLE		C OF BRGS. BEG. ABUT.					C OF BRGS. END. ABUT.				
		0.25 L	0.50 L	0.75 L							
GIRDERS 1 & 6	I STEEL D.L. (FT)	0.000	-0.004	-0.006	-0.004	0.000					
	II CONCRETE D.L. (FT)	0.000	-0.032	-0.045	-0.032	0.000					
	III SUPERIMPOSED D.L. (FT)	0.000	-0.003	-0.004	-0.003	0.000					
	IV VERTICAL CURVE (FT)	0.000	0.000	0.000	0.000	0.000					
	TOTAL = I+II+III+IV (FT)	0.000	-0.039	-0.054	-0.039	0.000					
GIRDERS 2, 3, 4, & 5	I STEEL D.L. (FT)	0.000	-0.004	-0.006	-0.004	0.000					
	II CONCRETE D.L. (FT)	0.000	-0.031	-0.044	-0.031	0.000					
	III SUPERIMPOSED D.L. (FT)	0.000	-0.003	-0.004	-0.003	0.000					
	IV VERTICAL CURVE (FT)	0.000	0.000	0.000	0.000	0.000					
	TOTAL = I+II+III+IV (FT)	0.000	-0.038	-0.054	-0.038	0.000					

DESIGN LOAD TABLE				
	UNIT	LOAD (K/ft.)	REACTION AT ABUTMENT (KIPS)	
GIRDERS 1 & 6	D.L.	SLAB	0.62	11.1
		HAUNCH	0.07	1.2
		GIRDER	0.08	1.5
		DIAPHRAGMS	0.01	0.2
		TOTAL:	0.78	14.0
GIRDERS 2, 3, 4, & 5	D.L.	SLAB	0.66	11.8
		HAUNCH	0.02	0.4
		GIRDER	0.08	1.5
		DIAPHRAGMS	0.02	0.3
		TOTAL:	0.78	14.0
GIRDERS 1 & 6	S.D.L.	OVERLAY	0.02	0.4
		RAILING	0.03	0.5
		FUTURE W.S.	0.10	1.8
		TOTAL:	0.15	2.7
		HL-93	-	52.6
GIRDERS 2, 3, 4, & 5	S.D.L.	OVERLAY	0.02	0.4
		RAILING	0.03	0.5
		FUTURE W.S.	0.10	1.8
		TOTAL:	0.15	2.7
		HL-93	-	43.6



CAMBER DIAGRAM - SINGLE SPAN
 (NOT TO SCALE)

CAMBER NOTES:

1. THE TOTAL CAMBER TOLERANCE FOR THESE GIRDERS IS +3/4", -0". THE CAMBER SHALL BE CHECKED IN THE VERTICAL POSITION (FULL MINUS STEEL GIRDER ONLY DEAD LOAD) IN THE FABRICATION SHOP BY THE FABRICATOR, AND IN THE FIELD IN THE VERTICAL POSITION AFTER ERECTION BY THE CONTRACTOR (FULL MINUS STEEL GIRDER AND DIAPHRAGM DEAD LOAD). IN THE FABRICATION SHOP, EACH GIRDER LINE SHALL BE FULLY ASSEMBLED (WITH AN APPROPRIATE NUMBER OF FASTENERS AT EACH BOLTED SPLICE CONNECTION TO DEVELOP THE SPLICE UNDER FULL GIRDER DEAD LOAD), SUPPORTED ONLY AT BEARING LOCATIONS, AND LATERALLY BRACED, PRIOR TO CHECKING THE CAMBER. THE CONTRACTOR WILL NOT BE ALLOWED ANY ADDITIONAL CAMBER TOLERANCE BEYOND THAT SHOWN ABOVE IN THE FIELD. REFER TO THE ERECTION/CAMBER MONITORING PROCEDURE ON THE STEEL FRAMING PLAN SHEET. ALL CAMBER REQUIREMENTS SHALL BE STRICTLY ENFORCED AND CERTIFIED BY THE SHOP AND FIELD INSPECTORS. CAMBER CHECKS WITH THE GIRDERS IN THE FLAT POSITION MAY BE DONE AT THE FABRICATOR'S OPTION, BUT ARE NOT REQUIRED. CAMBER CHECKS IN THE FLAT POSITION WILL NOT BE CONSIDERED AS A SUBSTITUTION FOR THE VERTICAL CHECKS.
2. THE CAMBER LABELED "VERTICAL CURVE" IN THE TABLE IS THE CAMBER REQUIRED TO FOLLOW THE VERTICAL CURVE.
3. THE CAMBER LABELED "STEEL D.L." IN THE TABLE IS THE CAMBER REQUIRED TO OFFSET THE DEFLECTION DUE TO THE DEAD LOAD WEIGHT OF THE GIRDER AND DIAPHRAGMS ONLY AS FABRICATED.
4. THE CAMBER LABELED "CONCRETE D.L." IN THE TABLE IS THE CAMBER REQUIRED TO OFFSET THE DEFLECTION DUE TO THE DEAD LOAD WEIGHT OF THE CONCRETE SLAB.
5. THE CAMBER LABELED "SUPERIMPOSED D.L." IN THE TABLE IS THE CAMBER REQUIRED TO OFFSET THE DEFLECTION DUE TO THE WEIGHT OF THE CURB, SIDEWALK, BARRIER, AND WEARING SURFACE.
6. THE TOTAL CAMBER IS THE SUM OF VERTICAL CURVE, STEEL DEAD LOAD, CONCRETE DEAD LOAD AND SUPERIMPOSED DEAD LOAD. ALL CAMBER OFFSETS ARE MEASURED VERTICALLY TO THE TOP OF THE WEB, FROM A STRAIGHT REFERENCE LINE DRAWN FROM THE INTERSECTION OF THE TOP OF THE WEB AND THE CENTERLINE OF THE BEARINGS AT ONE END OF THE GIRDER, TO THE CORRESPONDING POINT AT THE OTHER END OF THE GIRDER.
7. POSITIVE NUMBERS IN THE TABLE INDICATE VALUES ABOVE THE STRAIGHT REFERENCE LINE.
8. NEGATIVE NUMBERS IN THE TABLE INDICATE VALUES BELOW THE STRAIGHT REFERENCE LINE.
9. THE CAMBER OFFSETS ARE TABULATED IN FEET.

PREPARED BY: JEFFREY W. PANGBURN, P.E.
 1/10/2018
 ALTERED BY:
 ON:



AS-BUILT REVISIONS
 DESCRIPTION OF ALTERATIONS:

NY ROUTE 74 OVER EAGLE LAKE CHANNEL	PIN 1110.02
CULVERT REPLACEMENT	UTILITY QUALITY LEVEL "C"
SH 8230 SCHROON LAKE - TICONDEROGA, PART 2	BRIDGES 1080770
TOWN OF TICONDEROGA	CULVERTS C120086
COUNTY: ESSEX COUNTY	REGION: 1

ALL DIMENSIONS IN FT UNLESS OTHERWISE NOTED

CONTRACT NUMBER
 D264836
 DRAWING NO. ST-21
 SHEET NO. 44

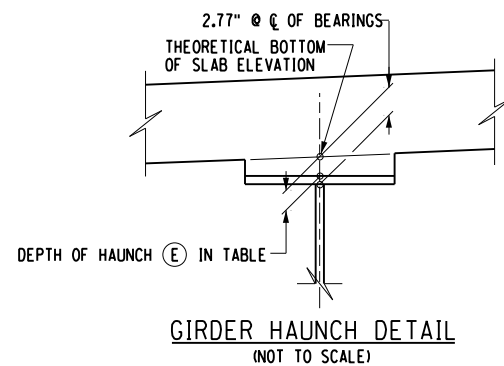
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DESIGN SUPERVISOR J. PANGBURN JOB MANAGER C. GEROUX DESIGN L. SHORT DRAFTING K. DETRICK CHECK A. BROWN CHECK A. BROWN PROJECT MANAGER C. GEROUX

HAUNCH TABLE						℄ OF BRGS. BEG. ABUT.	0.25 L	0.50 L	0.75 L	℄ OF BRGS. END. ABUT.
GIRDER 1	A	REQ'D BOTTOM OF SLAB ELEVATION	950.11	949.92	949.72	949.52	949.33			
	B	TOP OF STEEL EL. (FIELD MEASURE)								
	C	= A - B								
	D	CONCRETE + S.D.L DEFLECTION	0.00	-0.03	-0.05	-0.03	0.00			
	E	DEPTH OF HAUNCH REQ'D = C + D (FT)								
GIRDER 2	A	REQ'D BOTTOM OF SLAB ELEVATION	950.31	950.11	949.91	949.72	949.52			
	B	TOP OF STEEL EL. (FIELD MEASURE)								
	C	= A - B								
	D	CONCRETE + S.D.L DEFLECTION	0.00	-0.03	-0.05	-0.03	0.00			
	E	DEPTH OF HAUNCH REQ'D = C + D (FT)								
GIRDER 3	A	REQ'D BOTTOM OF SLAB ELEVATION	950.42	950.22	950.03	949.83	949.63			
	B	TOP OF STEEL EL. (FIELD MEASURE)								
	C	= A - B								
	D	CONCRETE + S.D.L DEFLECTION	0.00	-0.03	-0.05	-0.03	0.00			
	E	DEPTH OF HAUNCH REQ'D = C + D (FT)								
GIRDER 4	A	REQ'D BOTTOM OF SLAB ELEVATION	950.42	950.22	950.03	949.83	949.63			
	B	TOP OF STEEL EL. (FIELD MEASURE)								
	C	= A - B								
	D	CONCRETE + S.D.L DEFLECTION	0.00	-0.03	-0.05	-0.03	0.00			
	E	DEPTH OF HAUNCH REQ'D = C + D (FT)								
GIRDER 5	A	REQ'D BOTTOM OF SLAB ELEVATION	950.31	950.11	949.91	949.72	949.52			
	B	TOP OF STEEL EL. (FIELD MEASURE)								
	C	= A - B								
	D	CONCRETE + S.D.L DEFLECTION	0.00	-0.03	-0.05	-0.03	0.00			
	E	DEPTH OF HAUNCH REQ'D = C + D (FT)								
GIRDER 6	A	REQ'D BOTTOM OF SLAB ELEVATION	950.19	950.00	949.80	949.60	949.41			
	B	TOP OF STEEL EL. (FIELD MEASURE)								
	C	= A - B								
	D	CONCRETE + S.D.L DEFLECTION	0.00	-0.03	-0.05	-0.03	0.00			
	E	DEPTH OF HAUNCH REQ'D = C + D (FT)								

NOTE:
 THE CONTRACTOR SHALL PROVIDE THE ENGINEER WITH THE COMPLETED HAUNCH TABLE PRIOR TO SETTING THE BOTTOM FORMWORK OF THE SLAB TO DETERMINE APPROPRIATE TOP OF STEEL REFERENCE ELEVATIONS BASED ON THE ABUTMENT GEOMETRY AND THE HAUNCH DETAILS SHOWN ON THIS DRAWING.

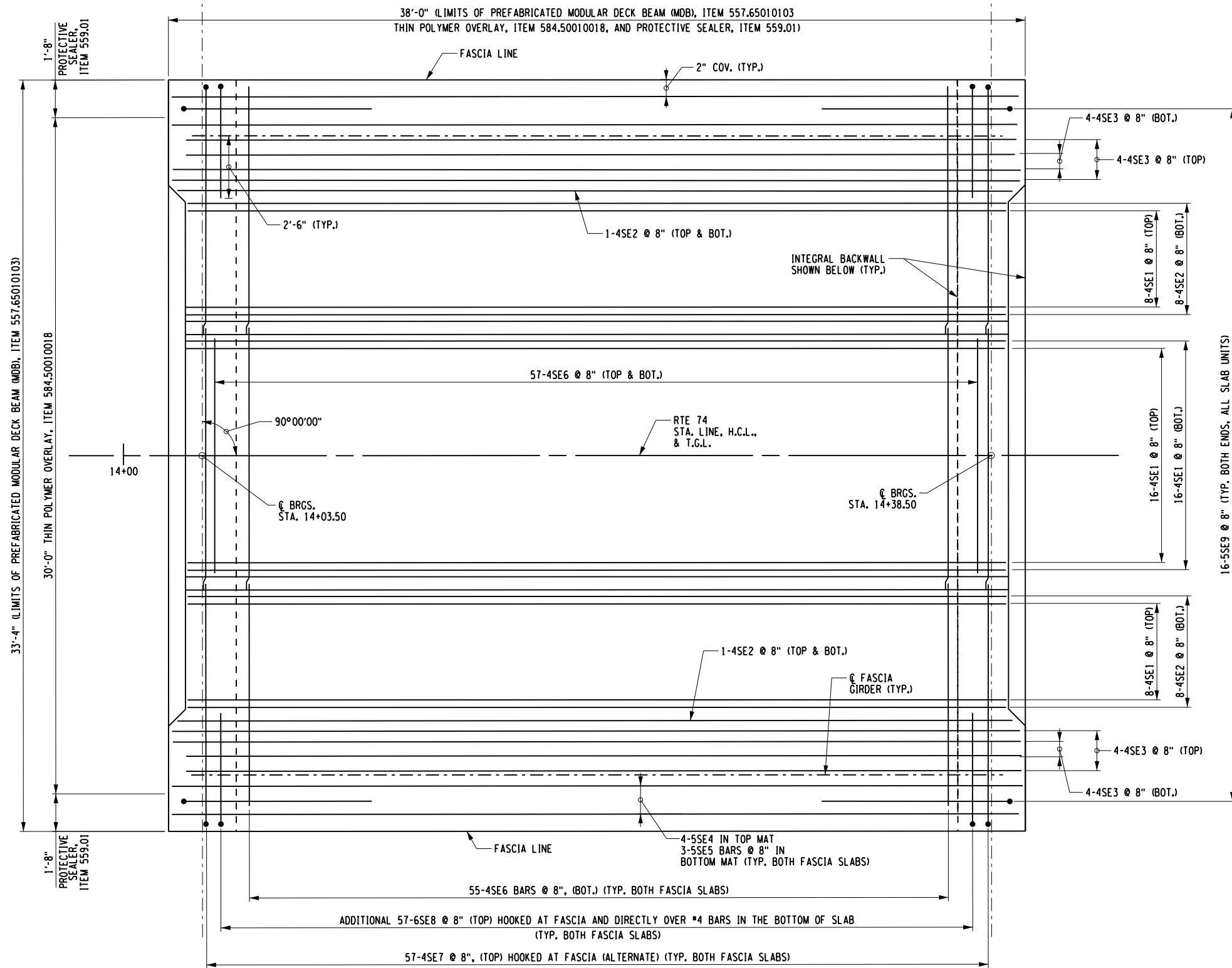


PREPARED BY: JEFFREY W. PANGBURN, P.E.	ALTERED BY: ON:

AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:	NY ROUTE 74 OVER EAGLE LAKE CHANNEL	PIN 1110.02	BRIDGES	CULVERTS	ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED	CONTRACT NUMBER	
	CULVERT REPLACEMENT	UTILITY QUALITY LEVEL "C"	1080770	C120086		D264836	
	SH 8230 SCHROON LAKE - TICONDEROGA, PART 2				HAUNCH TABLE	DRAWING NO. ST-22	
	TOWN OF TICONDEROGA					SHEET NO. 45	
	COUNTY: ESSEX COUNTY	REGION: 1					
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.							

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 USER: KDetrick
 PLOT: NYS DOT DME PDF.plt:fg

DESIGN SUPERVISOR: J. PANGBURN
 JOB MANAGER: C. GEROUX
 DESIGN: L. SHORT
 CHECK: A. BROWN
 DRAFTING: K. DETRICK
 CHECK: A. BROWN
 PROJECT MANAGER: C. GEROUX



SUPERSTRUCTURE SLAB REINFORCEMENT PLAN
 SCALE: 3/16"=1'-0"

SUPERSTRUCTURE SLAB TABLE		
PREFABRICATED MODULAR DECK BEAM (MDB) ITEM 557.65010103 (SF)	THIN POLYMER (EPOXY) OVERLAYS FOR STRUCTURAL SLABS ITEM 584.50010018 (SF)	PROTECTIVE SEALER OF NEW DECKS ITEM 559.01 (SF)
1232.00	1100.10	255.41

PREPARED BY: JEFFREY W. PANGBURN, P.E.
 ALTERED BY: ON:



AS-BUILT REVISIONS
 DESCRIPTION OF ALTERATIONS:

NY ROUTE 74 OVER EAGLE LAKE CHANNEL	PIN 1110.02
CULVERT REPLACEMENT	UTILITY QUALITY LEVEL "C"
SH 8230 SCHROON LAKE - TICONDEROGA, PART 2	BRIDGES 1080770
TOWN OF TICONDEROGA	CULVERTS C120086
COUNTY: ESSEX COUNTY	REGION: 1

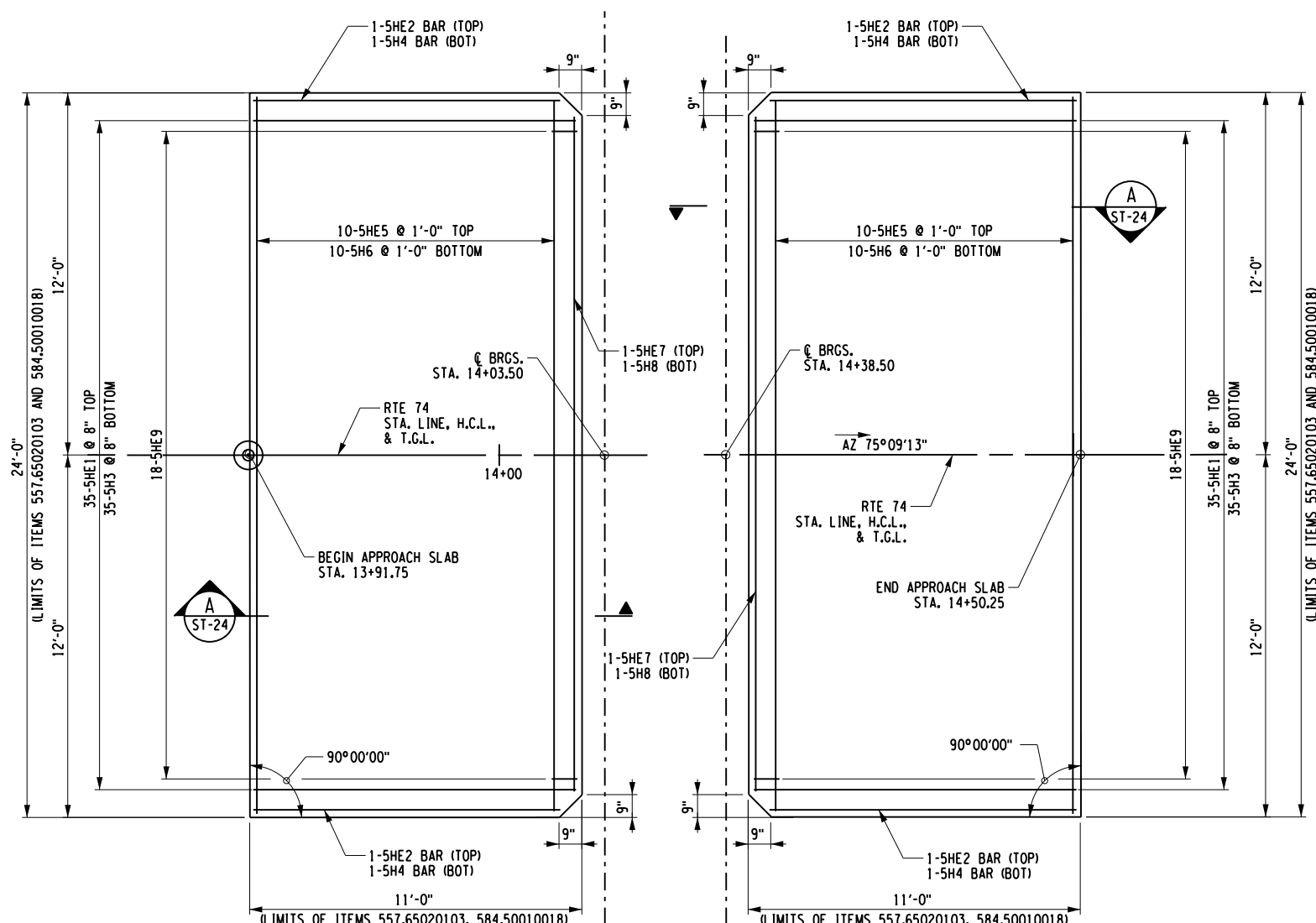
ALL DIMENSIONS IN FT UNLESS OTHERWISE NOTED	CONTRACT NUMBER D264836
SUPERSTRUCTURE SLAB PLAN	DRAWING NO. ST-23 SHEET NO. 46

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY, IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

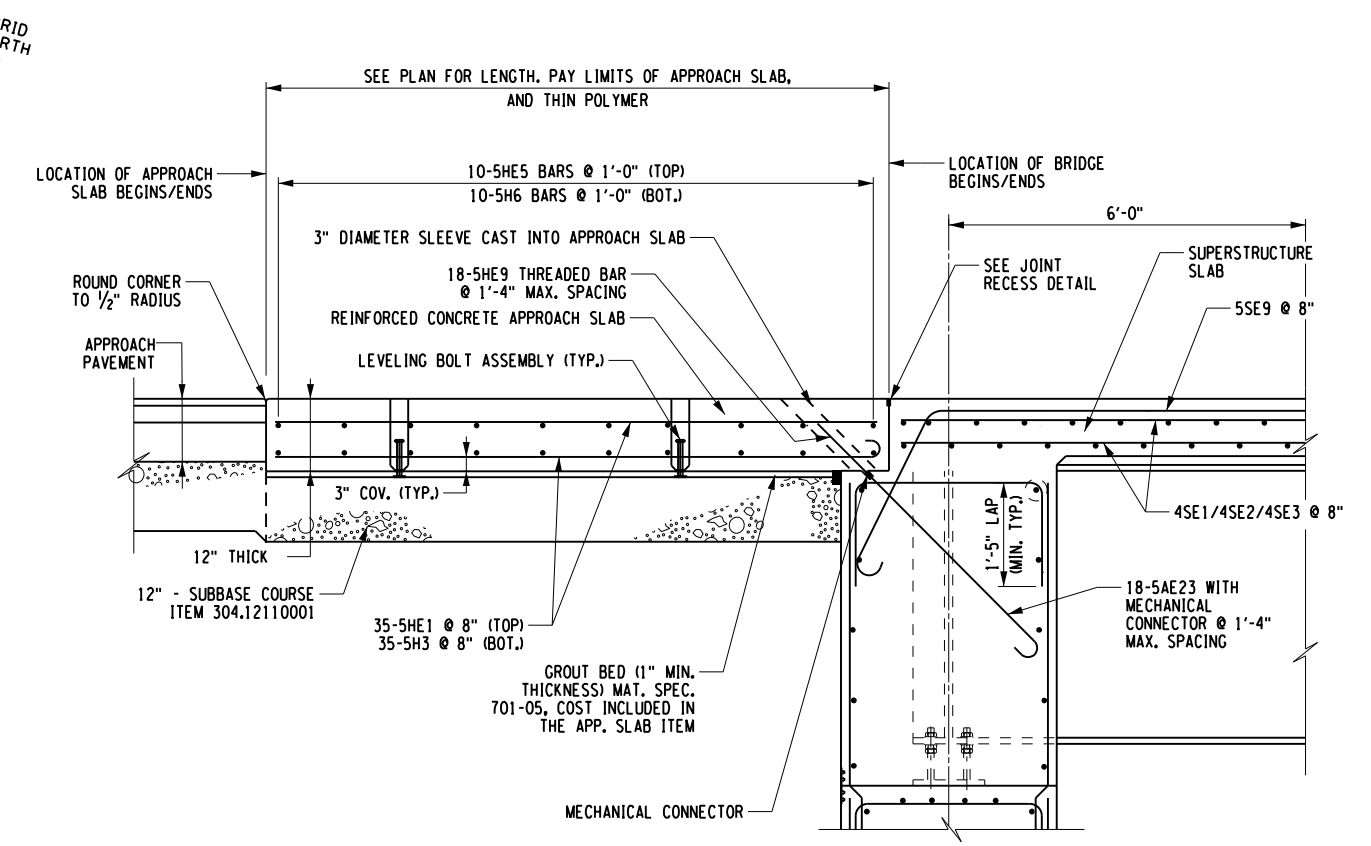


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 PLOT = NYS DOT CME PDF.plt:fg

DESIGN SUPERVISOR J. PANGBURN JOB MANAGER C. GEROUX DESIGN L. SHORT DRAFTING K. DETRICK CHECK A. BROWN CHECK A. BROWN PROJECT MANAGER C. GEROUX



APPROACH SLAB PLAN
 SCALE: 1/16"=1'-0"



A APPROACH SLAB SECTION
 ST-24 SCALE: 1/8"=1'-0"

APPROACH SLAB TABLE		
LOCATION	PRECAST CONCRETE APPROACH SLABS ITEM 557.65020103 (SF)	THIN POLYMER (EPOXY) OVERLAYS FOR STRUCTURAL SLABS ITEM 584.50010018 (SF)
BEGIN APPROACH SLAB	263.50	263.50
END APPROACH SLAB	263.50	263.50

- NOTES:
 1. ALL REINFORCEMENT SHALL HAVE 3" COVER UNLESS OTHERWISE NOTED.
 2. (E) DENOTES EPOXY COATED BARS.

PREPARED BY: JEFFREY W. PANGBURN, P.E. ALTERED BY: ON:



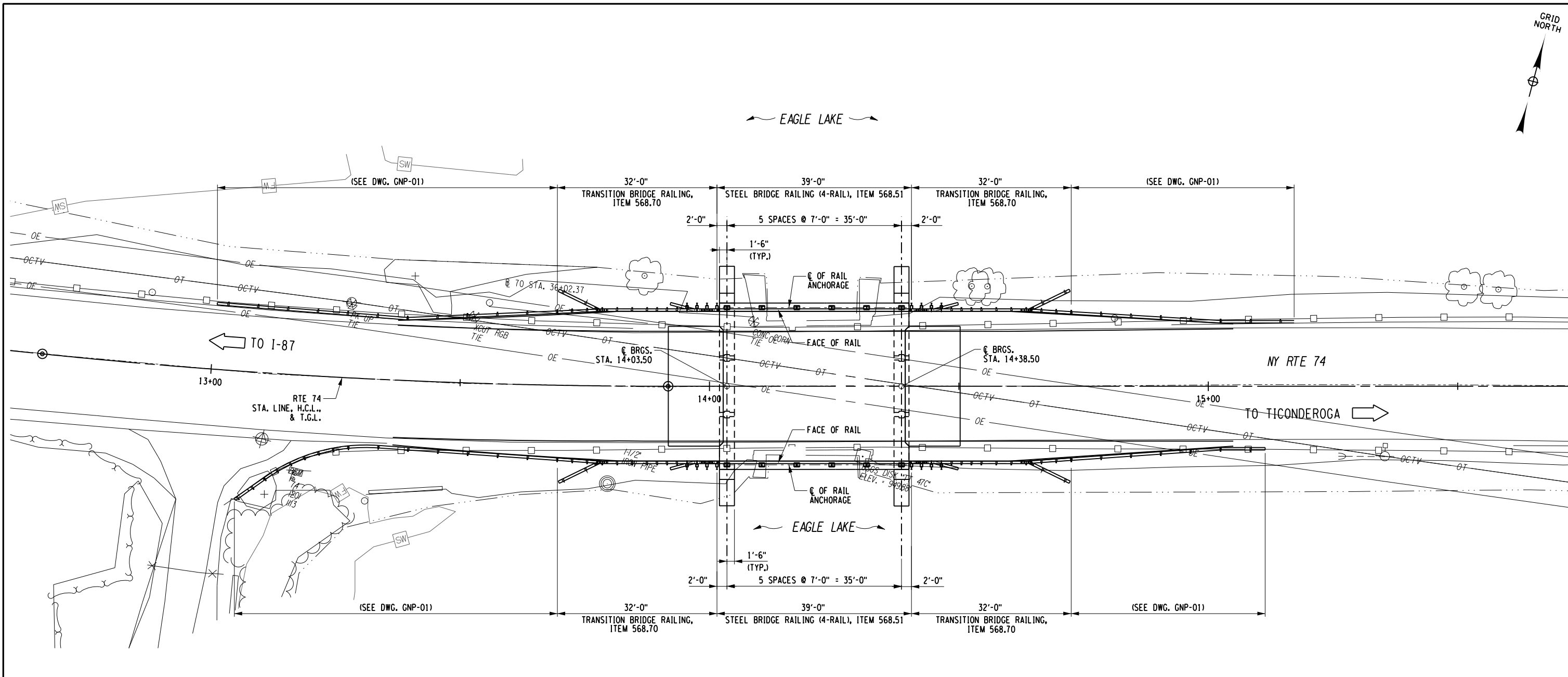
AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:	NY ROUTE 74 OVER EAGLE LAKE CHANNEL CULVERT REPLACEMENT SH 8230 SCHROON LAKE - TICONDEROGA, PART 2 TOWN OF TICONDEROGA COUNTY: ESSEX COUNTY REGION: 1	PIN 1110.02 UTILITY QUALITY LEVEL "C"	BRIDGES 1080770	CULVERTS C120086	ALL DIMENSIONS IN FT UNLESS OTHERWISE NOTED	CONTRACT NUMBER D264836
APPROACH SLAB PLAN					DRAWING NO. ST-24	SHEET NO. 47

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

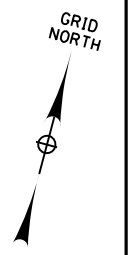


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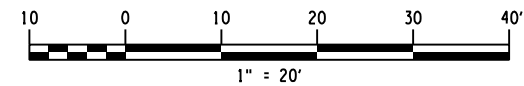
DESIGN SUPERVISOR J. PANGBURN
 JOB MANAGER C. GEROUX
 DESIGN L. SHORT
 CHECK A. BROWN
 DRAFTING K. DETRICK
 CHECK A. BROWN
 PROJECT MANAGER C. GEROUX



RAILING PLAN



PREPARED BY: JEFFREY W. PANGBURN, P.E. ALTERED BY: ON:



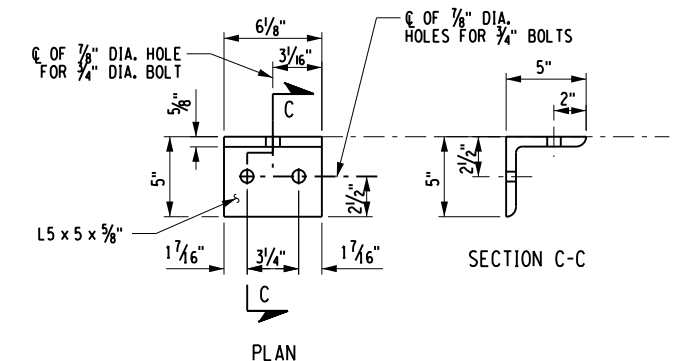
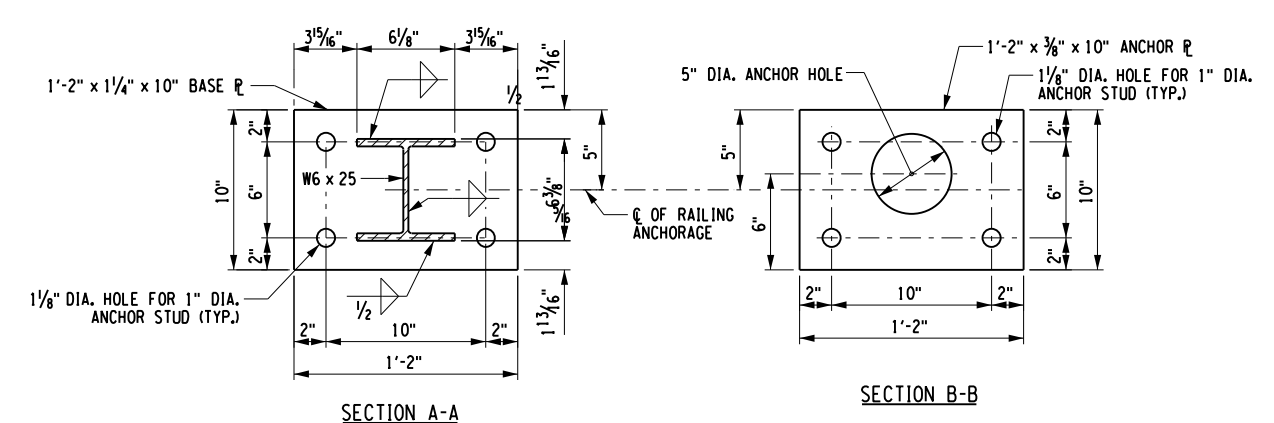
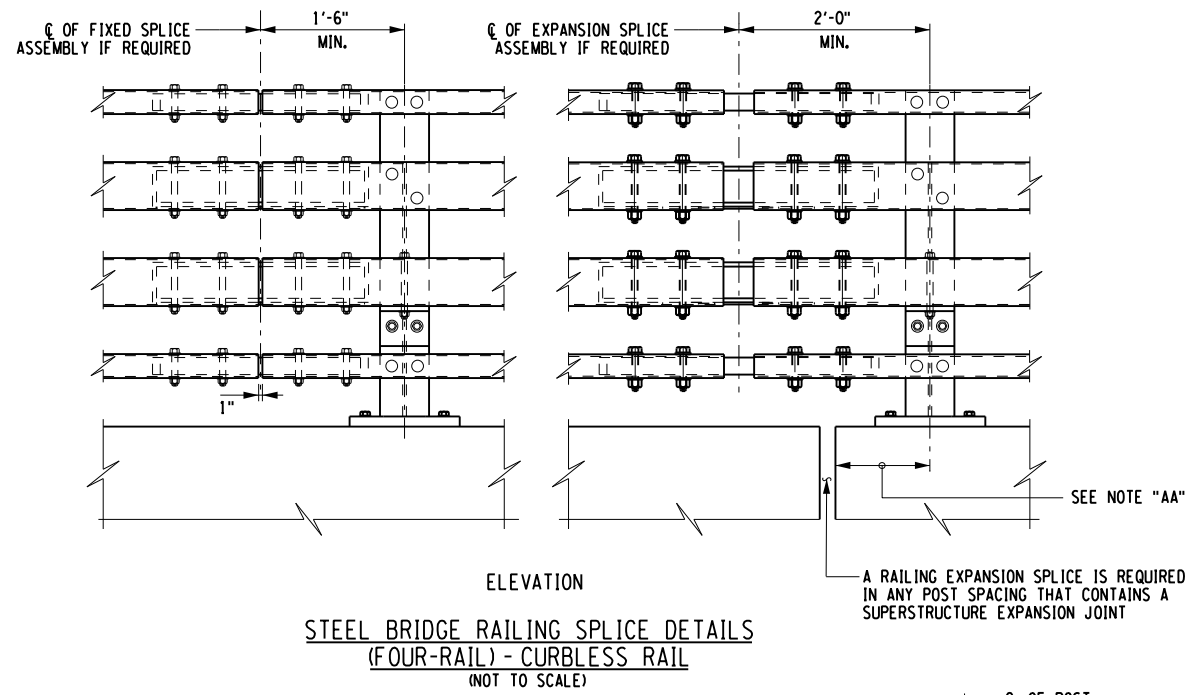
AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:	NY ROUTE 74 OVER EAGLE LAKE CHANNEL	PIN 1110.02	BRIDGES	CULVERTS	ALL DIMENSIONS IN FT UNLESS OTHERWISE NOTED	CONTRACT NUMBER
	CULVERT REPLACEMENT	UTILITY QUALITY LEVEL "C"	1080770	C120086		D264836
	SH 8230 SCHROON LAKE - TICONDEROGA, PART 2				BRIDGE RAILING PLAN	DRAWING NO. ST-25
	TOWN OF TICONDEROGA					SHEET NO. 48
	COUNTY: ESSEX COUNTY REGION: 1					

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

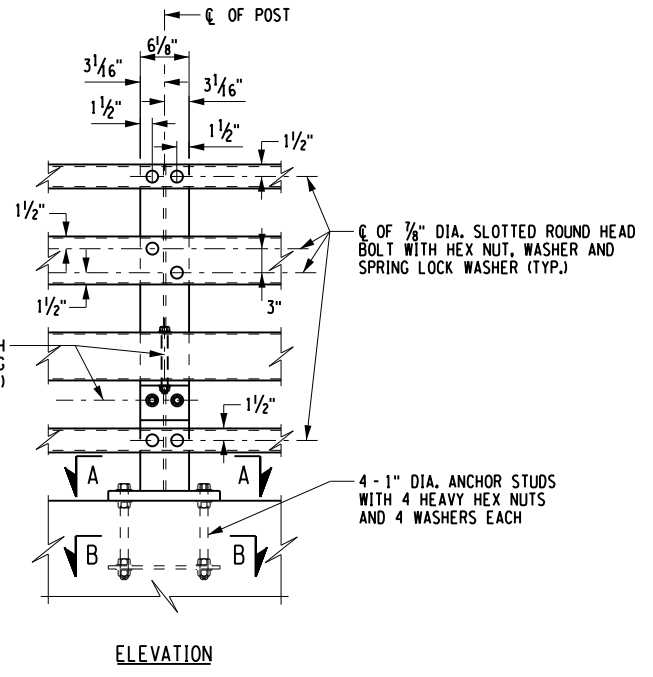
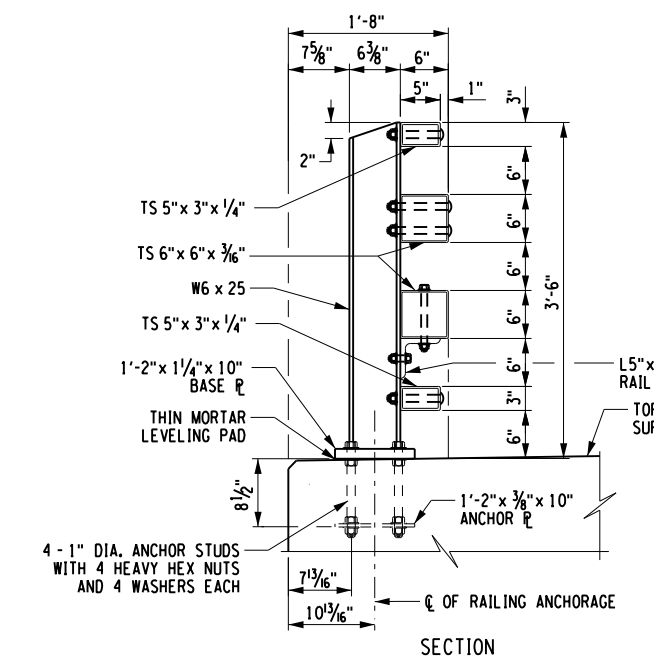
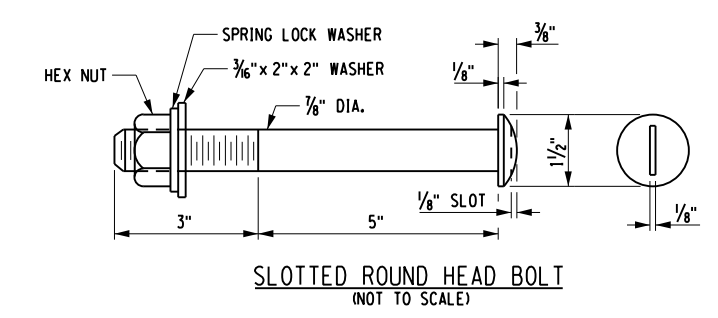


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 PLOT = NYS DOT CME PDF.plt:cfg

PROJECT MANAGER C. GEROUX
 CHECK A. BROWN
 DRAFTING K. DETRICK
 CHECK A. BROWN
 DESIGN L. SHORT
 JOB MANAGER C. GEROUX
 DESIGN SUPERVISOR J. PANGBURN



NOTES:
 ALL RAILING IS TO BE FABRICATED AND ERECTED ACCORDING TO SECTION 568 OF THE STANDARD SPECIFICATIONS.
 PRIOR TO GALVANIZING THE ASSEMBLED POST, GRIND ALL EDGES TO A MINIMUM RADIUS OF 1/16\"/>



STEEL BRIDGE RAILING (FOUR-RAIL) - CURBLESS (NOT TO SCALE)

NOTE "AA":
 THE MINIMUM DISTANCE FROM THE POST TO AN EXPANSION JOINT SHALL BE DETERMINED BY THE MINIMUM EDGE DISTANCE OF 5\"/>

PREPARED BY: JEFFREY W. PANGBURN, P.E. ALTERED BY: ON:



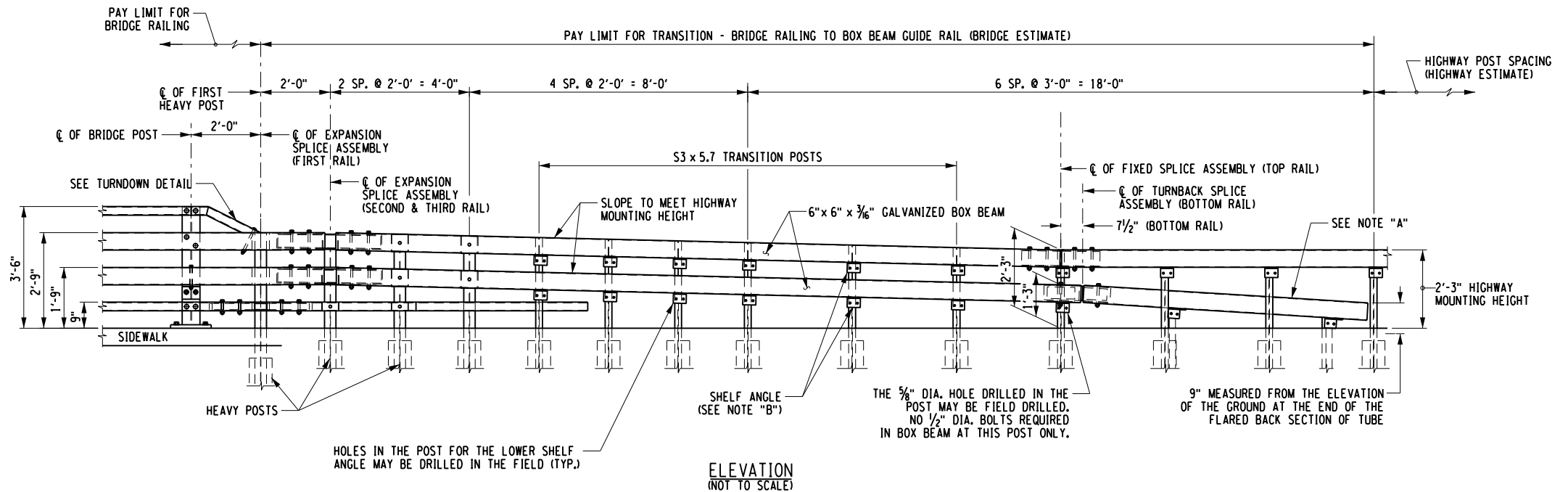
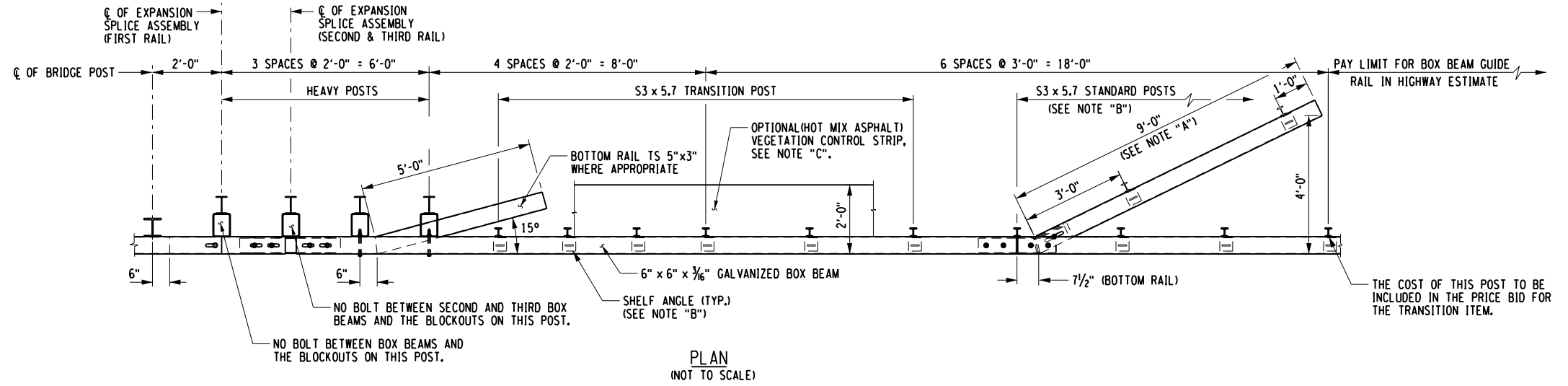
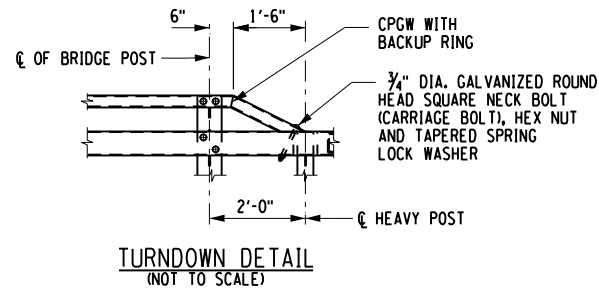
AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:	NY ROUTE 74 OVER EAGLE LAKE CHANNEL	PIN 1110.02	BRIDGES	CULVERTS	ALL DIMENSIONS IN FT UNLESS OTHERWISE NOTED	CONTRACT NUMBER
	CULVERT REPLACEMENT	UTILITY QUALITY LEVEL "C"	1080770	C120086		D264836
	SH 8230 SCHROON LAKE - TICONDEROGA, PART 2				BRIDGE RAILING DETAILS	DRAWING NO. ST-26
	TOWN OF TICONDEROGA					SHEET NO. 49
	COUNTY: ESSEX COUNTY	REGION: 1				

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY, IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.



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PROJECT MANAGER C. GEROUX
 CHECK A. BROWN
 DRAFTING K. DETRICK
 CHECK A. BROWN
 DESIGN L. SHORT
 JOB MANAGER C. GEROUX
 DESIGN SUPERVISOR J. PANGBURN



NOTES:
 NOTE "A": THE COST OF THE POSTS, SPLICE TUBE AND RAIL FOR THE LOWER TUBE FLARE SECTION IS INCLUDED IN THE PRICE BID FOR THE TRANSITION ITEM.
 NOTE "B": SEE TYPICAL RAIL TO POST CONNECTION DETAIL ON CURRENT HIGHWAY STANDARD SHEET TITLED "BOX BEAM GUIDE RAIL".
 NOTE "C": PAVE THIS AREA WITH THE SAME MATERIAL USED IN THE STABILIZED SHOULDER. PAYMENT WILL BE MADE UNDER THE SHOULDER MATERIAL ITEM (HIGHWAY ESTIMATE).

PREPARED BY: JEFFREY W. PANGBURN, P.E. ALTERED BY: ON:



AS-BUILT REVISIONS
 DESCRIPTION OF ALTERATIONS:

NY ROUTE 74 OVER EAGLE LAKE CHANNEL
CULVERT REPLACEMENT
SH 8230 SCHROON LAKE - TICONDEROGA, PART 2
TOWN OF TICONDEROGA
COUNTY: ESSEX COUNTY REGION: 1

PIN 1110.02
 UTILITY QUALITY LEVEL "C"

BRIDGES
 1080770

CULVERTS
 C120086

ALL DIMENSIONS IN FT UNLESS OTHERWISE NOTED

TRANSITION RAIL PLAN AND ELEVATION

CONTRACT NUMBER
 D264836

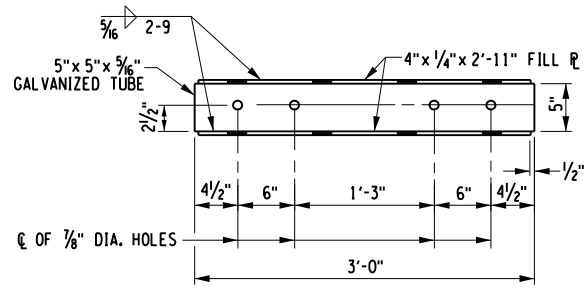
DRAWING NO. ST-27
 SHEET NO. 50

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY, IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

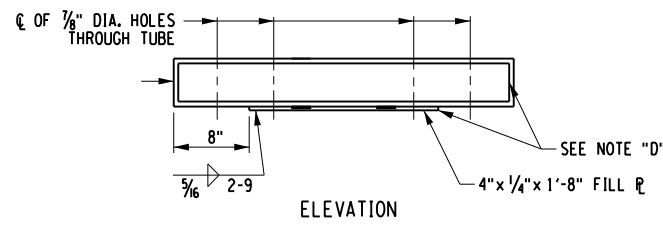


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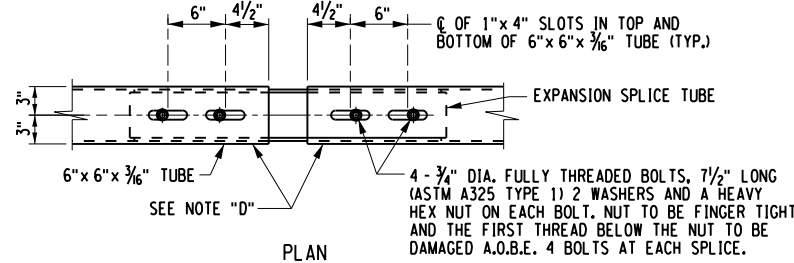
PROJECT MANAGER C. GEROUX
 CHECK A. BROWN
 DRAFTING K. DETRICK
 CHECK A. BROWN
 DESIGN L. SHORT
 JOB MANAGER C. GEROUX
 DESIGN SUPERVISOR J. PANGBURN



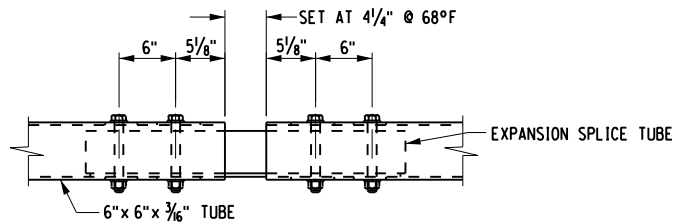
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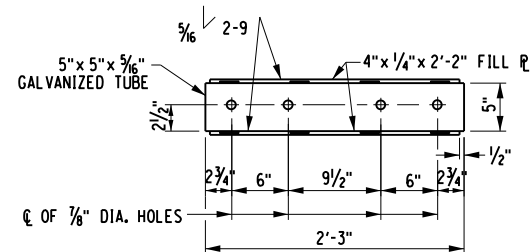
ELEVATION



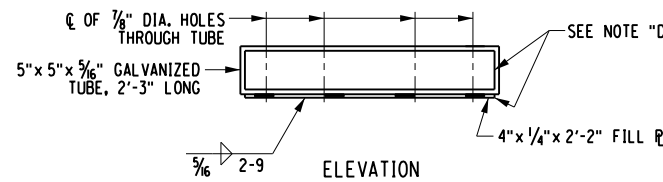
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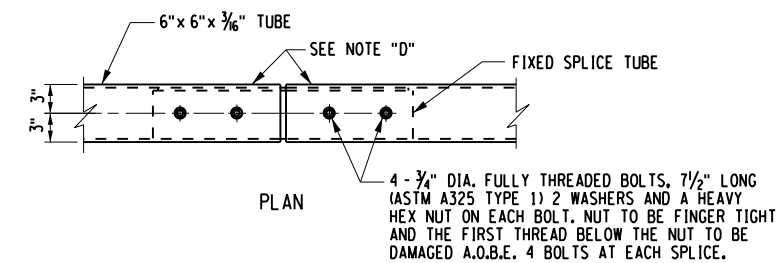
ELEVATION
EXPANSION SPLICE ASSEMBLY



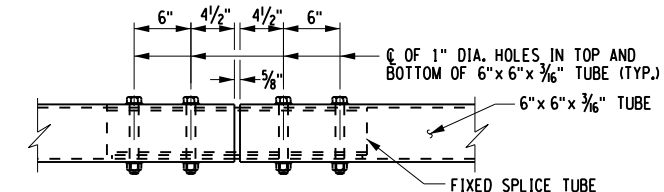
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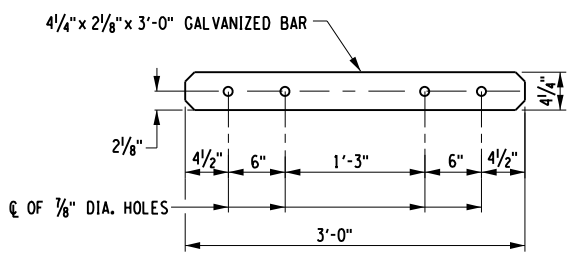
ELEVATION
FIXED SPLICE TUBE



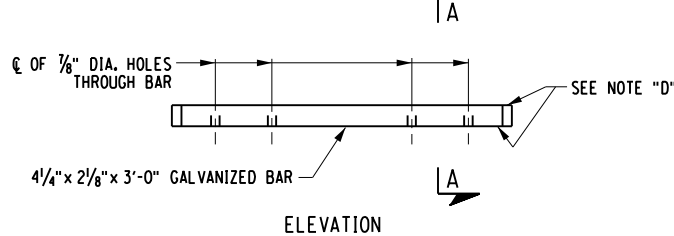
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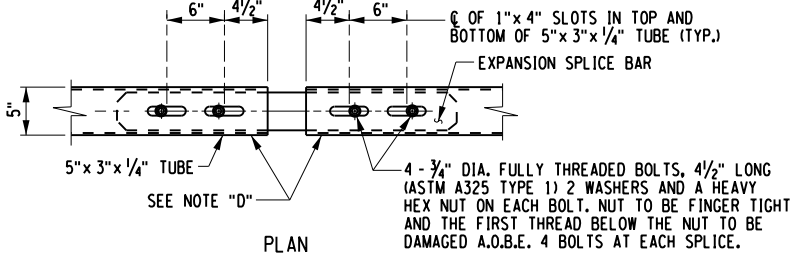
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FIXED SPLICE ASSEMBLY



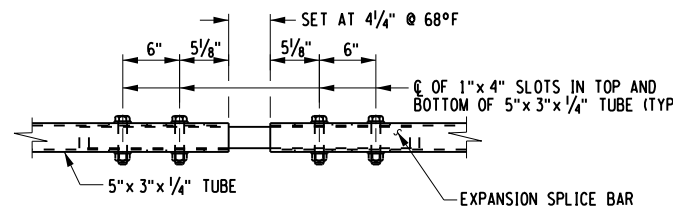
PLAN



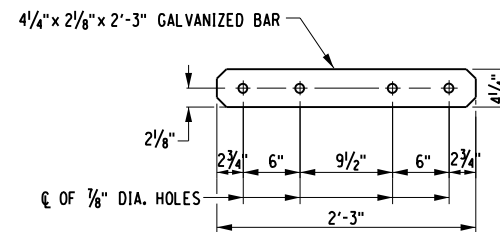
ELEVATION
EXPANSION SPLICE BAR



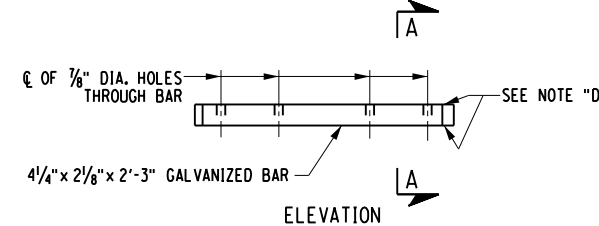
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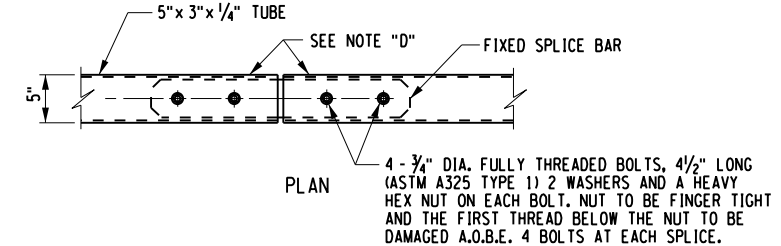
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EXPANSION SPLICE ASSEMBLY



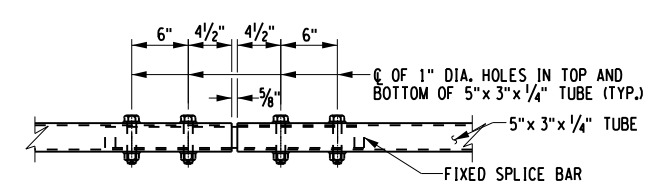
PLAN



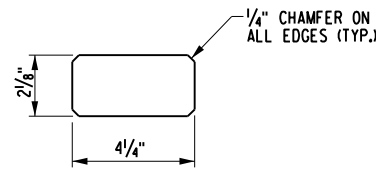
ELEVATION
FIXED SPLICE BAR



PLAN



ELEVATION
FIXED SPLICE ASSEMBLY



SECTION A-A

NOTES:
 NOTE "D": PROTRUSIONS CAUSED BY WELDING OR GALVANIZING ARE NOT PERMITTED ON THE ADJOINING SURFACES OF THE BOX BEAM RAILS, SPLICE TUBES AND FILL PLATES.
 DETAILS ON THE DRAWINGS LABELED AS "NOT TO SCALE" ARE INTENTIONALLY DRAWN NOT TO SCALE FOR VISUAL CLARITY. ALL OTHER DETAILS, FOR WHICH NO SCALE IS SHOWN, ARE DRAWN PROPORTIONAL AND ARE FULLY DIMENSIONED.

PREPARED BY: JEFFREY W. PANGBURN, P.E. ALTERED BY: ON:



AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:	NY ROUTE 74 OVER EAGLE LAKE CHANNEL	PIN 1110.02	BRIDGES	CULVERTS	ALL DIMENSIONS IN FT UNLESS OTHERWISE NOTED	CONTRACT NUMBER
	CULVERT REPLACEMENT	UTILITY QUALITY LEVEL "C"	1080770	C120086		D264836
	SH 8230 SCHROON LAKE - TICONDEROGA, PART 2					DRAWING NO. ST-28
	TOWN OF TICONDEROGA					SHEET NO. 51
	COUNTY: ESSEX COUNTY	REGION: 1				

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY, IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.




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 USER = KDetrick
 PLOT = NYS DOT CME PDF.plt:fg

DESIGN SUPERVISOR J. PANGBURN
 DESIGN L. SHORT
 CHECK A. BROWN
 DRAFTING K. DETRICK
 CHECK A. BROWN
 PROJECT MANAGER C. GEROUX

- NOTES:**
1. REINFORCEMENT FOR BOTH ABUTMENTS ARE EXACTLY THE SAME.
 2. REINFORCEMENT FOR BOTH APPROACH SLABS ARE EXACTLY THE SAME.




MARK	NO.	LENGTH	TYPE	WEIGHT	A	B	C	D	E	F	G	H H1	H2	J	K K1	K2	L	O	R
BEGIN ABUTMENT																			
STEM POUR LT UNIT																			
6AE1	12	18'-7"	N1	334															18'-7"
5AE2	6	4'-6"	17	28		1'-0"	2'-6"	1'-0"											
6AE3	8	4'-10"	N1	58															4'-10"
'O' VARIES FROM 2'-8" TO 7'-0" (1 SET OF 8)																			
7AE4	5	12'-5"	N1	127															12'-5"
8AE5	9	11'-7"	17	278		4'-5"	2'-8"	4'-5"											
8AE6	5	9'-6"	17	127		3'-5"	2'-8"	3'-5"											
8AE7	14	6'-4"	17	237		1'-10"	2'-8"	1'-10"											
8AE8	4	15'-3"	17	163		6'-4"	2'-8"	6'-4"											
8AE9	11	12'-10"	17	377		5'-1"	2'-8"	5'-1"											
'B' & 'D' VARY FROM 4'-0" TO 6'-3" (1 SET OF 22)																			
8AE10	15	6'-4"	17	254		1'-10"	2'-8"	1'-10"											
6AE11	4	3'-0"	N1	18															3'-0"
6AE12	4	4'-1"	N1	25															4'-1"
6AE13	4	10'-1"	N20	61		5'-9"	1'-8"	2'-8"				2'-5"	2'-8"		5'-3"				
5AE14	2	18'-7"	N1	39															18'-7"
EPOXY BAR SUBTOTAL				2,126	LB														
STEM POUR MID UNIT																			
6AE15	12	11'-11"	N1	216															11'-11"
8AE5	13	11'-7"	17	402		4'-5"	2'-8"	4'-5"											
8AE6	2	9'-6"	17	51		3'-5"	2'-8"	3'-5"											
8AE7	15	6'-4"	17	254		1'-10"	2'-8"	1'-10"											
6AE16	8	1'-8"	N1	20															1'-8"
6AE12	4	4'-1"	N1	25															4'-1"
5AE17	2	11'-11"	N1	25															11'-11"
EPOXY BAR SUBTOTAL				992	LB														
STEM POUR RT UNIT																			
6AE1	12	18'-7"	N1	334															18'-7"
5AE2	6	4'-6"	17	28		1'-0"	2'-6"	1'-0"											
6AE3	8	4'-10"	N1	58															4'-10"
'O' VARIES FROM 2'-8" TO 7'-0" (1 SET OF 8)																			
7AE4	5	12'-5"	N1	127															12'-5"
8AE5	9	11'-7"	17	278		4'-5"	2'-8"	4'-5"											
8AE6	5	9'-6"	17	127		3'-5"	2'-8"	3'-5"											
8AE7	14	6'-4"	17	237		1'-10"	2'-8"	1'-10"											
8AE8	4	15'-3"	17	163		6'-4"	2'-8"	6'-4"											
8AE9	11	12'-10"	17	377		5'-1"	2'-8"	5'-1"											
'B' & 'D' VARY FROM 4'-0" TO 6'-3" (1 SET OF 22)																			
8AE10	15	6'-4"	17	254		1'-10"	2'-8"	1'-10"											
6AE11	4	3'-0"	N1	18															3'-0"
6AE12	4	4'-1"	N1	25															4'-1"
6AE13	4	10'-1"	N20	61		5'-9"	1'-8"	2'-8"				2'-5"	2'-8"		5'-3"				
5AE14	2	18'-7"	N1	39															18'-7"
EPOXY BAR SUBTOTAL				2,126	LB														
BACKWALL POUR LT UNIT																			
6AE18	7	11'-2"	N1	118															11'-2"
6AE19	1	2'-3"	N1	3															2'-3"
6AE20	1	5'-5"	N1	8															5'-5"
6AE21	1	2'-12"	N1	4															2'-12"
8AE22	18	5'-10"	17	282		1'-8"	2'-6"	1'-8"											
5AE23	5	3'-5"	1	18		0'-7"	2'-10"	INCLUDING LENGTH OF COUPLER				0'-0"			0'-5"				
EPOXY BAR SUBTOTAL				434	LB														

PREPARED BY: JEFFREY W. PANGBURN, P.E. ALTERED BY: ON:



AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:	NY ROUTE 74 OVER EAGLE LAKE CHANNEL	PIN 1110.02	BRIDGES	CULVERTS	ALL DIMENSIONS IN FT UNLESS OTHERWISE NOTED	CONTRACT NUMBER D264836
	CULVERT REPLACEMENT	UTILITY QUALITY LEVEL "C"	1080770	C120086		
	SH 8230 SCHROON LAKE - TICONDEROGA, PART 2					
	TOWN OF TICONDEROGA				BAR LIST - 1	DRAWING NO. ST-29 SHEET NO. 52
	COUNTY: ESSEX COUNTY REGION: 1					

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY, IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

FILE NAME = N:\Projects\2020\126-209 NYS DOT - Eagle Lake\Working\CADD\egon\111002.cpl.tbl.bar_02.dgn
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 USER = KDetrick
 PLOT = NYS DOT CME PDF.plt:cjg

DESIGN SUPERVISOR J. PANGBURN JOB MANAGER C. GEROUX DESIGN L. SHORT DRAFTING K. DETRICK CHECK A. BROWN PROJECT MANAGER C. GEROUX

MARK	NO.	LENGTH	TYPE	WEIGHT	A	B	C	D	E	F	G	H H1	H2	J	K K1	K2	L	O	R
BEGIN ABUTMENT																			
BACKWALL POUR MID UNIT																			
6AE24	7	11'-11"	N1	125														11'-11"	
6AE21	2	2'-12"	N1	9														2'-12"	
6AE20	1	5'-5"	N1	8														5'-5"	
8AE22	17	5'-10"	17	267		1'-8"	2'-6"	1'-8"											
5AE23	8	3'-5"	1	29	0'-7"	2'-10"	INCLUDING	LENGTH OF	COUPLER		0'-0"			0'-5"					
EPOXY BAR SUBTOTAL				438	LB														
BACKWALL POUR RT UNIT																			
6AE18	7	11'-2"	N1	118														11'-2"	
6AE19	1	2'-3"	N1	3														2'-3"	
6AE20	1	5'-5"	N1	8														5'-5"	
6AE21	1	2'-12"	N1	4														2'-12"	
8AE22	18	5'-10"	17	282		1'-8"	2'-6"	1'-8"											
5AE23	5	3'-5"	1	18	0'-7"	2'-10"	INCLUDING	LENGTH OF	COUPLER		0'-0"			0'-5"					
EPOXY BAR SUBTOTAL				434	LB														
END ABUTMENT																			
STEM POUR LT UNIT																			
6AE1	12	18'-7"	N1	334														18'-7"	
5AE2	6	4'-6"	17	28		1'-0"	2'-6"	1'-0"											
6AE3	8	4'-10"	N1	58														4'-10"	
'O' VARIES FROM 2'-8" TO 7'-0" (1 SET OF 8)																			
7AE4	5	12'-5"	N1	127														12'-5"	
8AE5	9	11'-7"	17	278		4'-5"	2'-8"	4'-5"											
8AE6	5	9'-6"	17	127		3'-5"	2'-8"	3'-5"											
8AE7	14	6'-4"	17	237		1'-10"	2'-8"	1'-10"											
8AE8	4	15'-3"	17	163		6'-4"	2'-8"	6'-4"											
8AE9	11	12'-10"	17	377		5'-1"	2'-8"	5'-1"											
'B' & 'D' VARY FROM 4'-0" TO 6'-3" (1 SET OF 22)																			
8AE10	15	6'-4"	17	254		1'-10"	2'-8"	1'-10"											
6AE11	4	3'-0"	N1	18														3'-0"	
6AE12	4	4'-1"	N1	25														4'-1"	
6AE13	4	10'-1"	N20	61		5'-9"	1'-8"	2'-8"			2'-5"	2'-8"		5'-3"					
5AE14	2	18'-7"	N1	39														18'-7"	
EPOXY BAR SUBTOTAL				2,126	LB														
STEM POUR MID UNIT																			
6AE15	12	11'-11"	N1	216														11'-11"	
8AE5	13	11'-7"	17	402		4'-5"	2'-8"	4'-5"											
8AE6	2	9'-6"	17	51		3'-5"	2'-8"	3'-5"											
8AE7	15	6'-4"	17	254		1'-10"	2'-8"	1'-10"											
6AE16	8	1'-8"	N1	20														1'-8"	
6AE12	4	4'-1"	N1	25														4'-1"	
5AE17	2	11'-11"	N1	25														11'-11"	
EPOXY BAR SUBTOTAL				992	LB														
STEM POUR RT UNIT																			
6AE1	12	18'-7"	N1	334														18'-7"	
5AE2	6	4'-6"	17	28		1'-0"	2'-6"	1'-0"											
6AE3	8	4'-10"	N1	58														4'-10"	
'O' VARIES FROM 2'-8" TO 7'-0" (1 SET OF 8)																			
7AE4	5	12'-5"	N1	127														12'-5"	
8AE5	9	11'-7"	17	278		4'-5"	2'-8"	4'-5"											
8AE6	5	9'-6"	17	127		3'-5"	2'-8"	3'-5"											
8AE7	14	6'-4"	17	237		1'-10"	2'-8"	1'-10"											
8AE8	4	15'-3"	17	163		6'-4"	2'-8"	6'-4"											
8AE9	11	12'-10"	17	377		5'-1"	2'-8"	5'-1"											
'B' & 'D' VARY FROM 4'-0" TO 6'-3" (1 SET OF 22)																			
8AE10	15	6'-4"	17	254		1'-10"	2'-8"	1'-10"											
6AE11	4	3'-0"	N1	18														3'-0"	
6AE12	4	4'-1"	N1	25														4'-1"	
6AE13	4	10'-1"	N20	61		5'-9"	1'-8"	2'-8"			2'-5"	2'-8"		5'-3"					
5AE14	2	18'-7"	N1	39														18'-7"	
EPOXY BAR SUBTOTAL				2,126	LB														

PREPARED BY: JEFFREY W. PANGBURN, P.E. ALTERED BY: ON:



AS-BUILT REVISIONS
DESCRIPTION OF ALTERATIONS:

NY ROUTE 74 OVER EAGLE LAKE CHANNEL
 CULVERT REPLACEMENT
 SH 8230 SCHROON LAKE - TICONDEROGA, PART 2
 TOWN OF TICONDEROGA
 COUNTY: ESSEX COUNTY REGION: 1

PIN 1110.02
UTILITY QUALITY LEVEL "C"

BRIDGES
1080770

CULVERTS
C120086

ALL DIMENSIONS IN FT UNLESS OTHERWISE NOTED

CONTRACT NUMBER
D264836

BAR LIST - 2

DRAWING NO. ST-30
SHEET NO. 53

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.



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 PLOT = NYS DOT CME PDF.plt:fg

DESIGN SUPERVISOR J. PANGBURN JOB MANAGER C. GEROUX DRAFTING K. DETRICK CHECK A. BROWN PROJECT MANAGER C. GEROUX

MARK	NO.	LENGTH	TYPE	WEIGHT	A	B	C	D	E	F	G	H H1	H2	J	K K1	K2	L	O	R
END ABUTMENT																			
BACKWALL POUR LT UNIT																			
6AE18	7	11'-2"	N1	118															11'-2"
6AE19	1	2'-3"	N1	3															2'-3"
6AE20	1	5'-5"	N1	8															5'-5"
6AE21	1	2'-12"	N1	4															2'-12"
8AE22	18	5'-10"	17	282		1'-8"	2'-6"	1'-8"											
5AE23	5	3'-5"	1	18	0'-7"	2'-10"	INCLUDING	LENGTH OF	COUPLER		0'-0"			0'-5"					
EPOXY BAR SUBTOTAL				434	LB														
BACKWALL POUR MID UNIT																			
6AE24	7	11'-11"	N1	125															11'-11"
6AE21	2	2'-12"	N1	9															2'-12"
6AE20	1	5'-5"	N1	8															5'-5"
8AE22	17	5'-10"	17	267		1'-8"	2'-6"	1'-8"											
5AE23	8	3'-5"	1	29	0'-7"	2'-10"	INCLUDING	LENGTH OF	COUPLER		0'-0"			0'-5"					
EPOXY BAR SUBTOTAL				438	LB														
BACKWALL POUR RT UNIT																			
6AE18	7	11'-2"	N1	118															11'-2"
6AE19	1	2'-3"	N1	3															2'-3"
6AE20	1	5'-5"	N1	8															5'-5"
6AE21	1	2'-12"	N1	4															2'-12"
8AE22	18	5'-10"	17	282		1'-8"	2'-6"	1'-8"											
5AE23	5	3'-5"	1	18	0'-7"	2'-10"	INCLUDING	LENGTH OF	COUPLER		0'-0"			0'-5"					
EPOXY BAR SUBTOTAL				434	LB														
SUPERSTRUCTURE SLAB																			
LT SLAB UNIT																			
4SE1	16	36'-3"	N1	388															36'-3"
4SE2	2	37'-1"	N1	50															37'-1"
4SE3	8	37'-9"	N1	202															37'-9"
5SE4	4	37'-9"	N1	158															37'-9"
5SE5	3	37'-9"	N1	118															37'-9"
4SE6	55	11'-9"	N1	432															11'-9"
4SE7	57	12'-3"	1	467	0'-6"	11'-9"				0'-0"				0'-4"					
6SE8	57	5'-7"	1	475	0'-8"	4'-11"				0'-0"				0'-6"					
5SE9	16	9'-10"	11	164	0'-7"	3'-2"	0'-1"	6'-0"											0'-1"
EPOXY BAR SUBTOTAL				2,452	LB														
MIDDLE SLAB UNIT																			
4SE1	32	36'-3"	N1	775															36'-3"
4SE6	112	11'-1"	N1	829															11'-1"
5SE9	16	9'-10"	N1	164	0'-7"	3'-2"	0'-1"	6'-0"											0'-1"
EPOXY BAR SUBTOTAL				1,769	LB														
RT SLAB UNIT																			
4SE1	16	36'-3"	N1	388															36'-3"
4SE2	2	37'-1"	N1	50															37'-1"
4SE3	8	37'-9"	N1	202															37'-9"
5SE4	4	37'-9"	N1	158															37'-9"
5SE5	3	37'-9"	N1	118															37'-9"
4SE6	55	11'-9"	N1	432															11'-9"
4SE7	57	12'-3"	1	467	0'-6"	11'-9"				0'-0"				0'-4"					
6SE8	57	5'-7"	1	475	0'-8"	4'-11"				0'-0"				0'-6"					
5SE9	16	9'-10"	11	164	0'-7"	3'-2"	0'-1"	6'-0"											0'-1"
EPOXY BAR SUBTOTAL				2,452	LB														

PREPARED BY: JEFFREY W. PANGBURN, P.E. ALTERED BY: ON:



AS-BUILT REVISIONS
 DESCRIPTION OF ALTERATIONS:

NY ROUTE 74 OVER EAGLE LAKE CHANNEL
 CULVERT REPLACEMENT
 SH 8230 SCHROON LAKE - TICONDEROGA, PART 2
 TOWN OF TICONDEROGA
 COUNTY: ESSEX COUNTY REGION: 1

PIN 1110.02
 UTILITY QUALITY LEVEL "C"
 BRIDGES
 1080770
 CULVERTS
 C120086

ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED
 BAR LIST - 3

CONTRACT NUMBER
 D264836
 DRAWING NO. ST-31
 SHEET NO. 54

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY, IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.



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DESIGN SUPERVISOR J. PANGBURN JOB MANAGER C. GEROUX DRAFTING K. DETRICK CHECK A. BROWN CHECK A. BROWN PROJECT MANAGER C. GEROUX

MARK	NO.	LENGTH	TYPE	WEIGHT	A	B	C	D	E	F	G	H H1	H2	J	K K1	K2	L	O	R
BEGIN APPROACH SLAB																			
5HE1	35	10'-9"	N1	393															10'-9"
5HE2	2	10'-2"	N1	21															10'-2"
5H3	35	11'-4"	1	414	0'-7"	10'-9"					0'-0"								
5H4	2	10'-9"	1	22	0'-7"	10'-2"					0'-0"								
5HE5	10	23'-9"	N1	248															23'-9"
5H6	10	23'-9"	N1	248															23'-9"
5HE7	1	22'-9"	N1	24															22'-9"
5H8	1	22'-9"	N1	24															22'-9"
5HE9	18	1'-11"	N1	36															1'-11"
EPOXY BAR SUBTOTAL				722	LB														
BLACK BAR SUBTOTAL				708	LB														
END APPROACH SLAB																			
5HE1	35	10'-9"	N1	393															10'-9"
5HE2	2	10'-2"	N1	21															10'-2"
5H3	35	11'-4"	1	414	0'-7"	10'-9"					0'-0"								
5H4	2	10'-9"	1	22	0'-7"	10'-2"					0'-0"								
5HE5	10	23'-9"	N1	248															23'-9"
5H6	10	23'-9"	N1	248															23'-9"
5HE7	1	22'-9"	N1	24															22'-9"
5H8	1	22'-9"	N1	24															22'-9"
5HE9	18	1'-11"	N1	36															1'-11"
EPOXY BAR SUBTOTAL				722	LB														
BLACK BAR SUBTOTAL				708	LB														

PREPARED BY: JEFFREY W. PANGBURN, P.E. ALTERED BY:
 ON:



AS-BUILT REVISIONS
 DESCRIPTION OF ALTERATIONS:

NY ROUTE 74 OVER EAGLE LAKE CHANNEL
 CULVERT REPLACEMENT
 SH 8230 SCHROON LAKE - TICONDEROGA, PART 2
 TOWN OF TICONDEROGA
 COUNTY: ESSEX COUNTY REGION: 1

PIN 1110.02
 UTILITY QUALITY LEVEL "C"
 BRIDGES 1080770
 CULVERTS C120086

ALL DIMENSIONS IN FT UNLESS OTHERWISE NOTED
 BAR LIST - 4

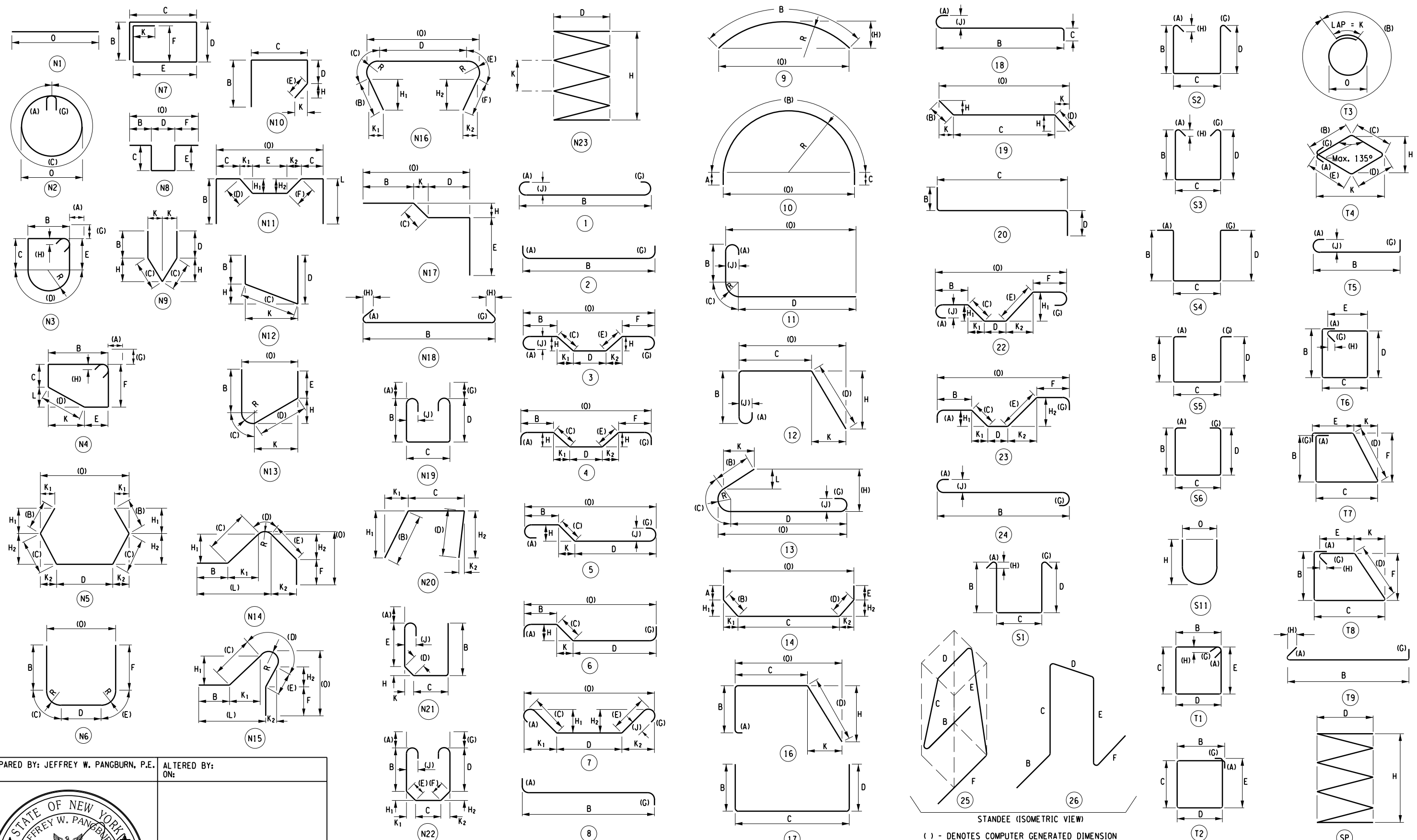
CONTRACT NUMBER D264836
 DRAWING NO. ST-32
 SHEET NO. 55

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.



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 DATE/TIME = 4/27/2022 2:56:51 PM
 USER = KDetrick
 PLOT = NYS DOT DME PDF plotter

DESIGN SUPERVISOR J. PANGBURN
 DESIGN L. SHORT
 CHECK A. BROWN
 DRAFTING K. DETRICK
 CHECK A. BROWN
 PROJECT MANAGER C. GEROUX



() - DENOTES COMPUTER GENERATED DIMENSION

PREPARED BY: JEFFREY W. PANGBURN, P.E.
 ALTERED BY: ON:



AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:	NY ROUTE 74 OVER EAGLE LAKE CHANNEL	PIN 1110.02	BRIDGES	CULVERTS	ALL DIMENSIONS IN FT UNLESS OTHERWISE NOTED	CONTRACT NUMBER
	CULVERT REPLACEMENT	UTILITY QUALITY LEVEL "C"	1080770	C120086		D264836
	SH 8230 SCHROON LAKE - TICONDEROGA, PART 2					
	TOWN OF TICONDEROGA					
	COUNTY: ESSEX COUNTY	REGION: 1	BAR BENDING DIAGRAM			DRAWING NO. ST-33
SHEET NO. 56						

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

